

ATTACHMENT B

Note:

The following information was commissioned by the Philadelphia Redevelopment Authority in 2016 in anticipation of this Request for Proposals. The preparer of this information, Maser Consulting, will be present at the RFP Pre-Submission Conference to answer questions that potential respondents have regarding its contents.



Engineers
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September 1, 2016

Mr. Robert LaBrum
Director, Design and Construction
Philadelphia Redevelopment Authority
1234 Market Street, 16th Floor
Philadelphia, PA 19107

Re: Geotechnical Review
Proposed Redevelopment – 800-30 Vine Street
Philadelphia, Pennsylvania
MC Project No. 15001540A

Dear Mr. LaBrum:

Maser Consulting P.A. is pleased to present this report of a geotechnical review performed for the proposed site redevelopment located at 800-30 Vine Street in Philadelphia, Pennsylvania, as shown on the attached Site Location Map, Figure 1.

The purpose of this evaluation was to obtain and review available information related to the site geology, as well as research the historic development and neighboring site features that could impact proposed development at the project site. As part of the review, we also performed one site visit on August 30, 2016 to visually observe for surface features that could impact the proposed development. A summary of our findings is provided in the following sections.

Existing Site Conditions

At the ground surface, the site generally consists of an on-grade, asphalt- and concrete-paved parking lot. Row home structures occupy a limited portion of the site in the southwest corner.

The Broad-Ridge Spur (of the Broad Street Line) and the Regional Rail Line (Center City Commuter Rail) subways intersect the site below-grade. The 8th Street Subway entrance is located in the southeastern quadrant of the site. Please refer to the attached Transit Map, Figure 3 for additional location information.

We have reviewed the “General Plan and Profile” drawings for Contract No. 301 (Race Street to Hamilton Street) and Contract No. 302 (Walnut Street to Race Street) of the Broad-Ridge Spur (see Appendix A). According to the plans, the subway extends about 20 to 25 feet below grade. The plans provided also show the location of several test borings completed for the subway.

Based on the Boundary and Topographic Survey plan by Maser Consulting, P.A., there are a significant number of above- and below-grade utilities located throughout the site, including



water, gas, electric, and telephone. Ventilation grates are also visible at the ground surface (associated with the subway tunnels).

Historic Site Development

Based on a review of historic aerial images, the site has a history of development and redevelopment. While we did not research information confirming the usage of the former development features (as would be done for a Phase I ESA), the historic development generally appears to consist of low-rise residential, commercial and/or retail type structures. We expect that remnants of these former structures will be present at the site (foundations, slabs, walls, demolition debris, etc.), including backfilled basements. Please refer to the historic aerial photographs included in Appendix B for additional information.

Ridge Avenue previously bisected the site from southeast to northwest. Ridge Avenue appears to have been re-routed (discontinued at 8th Street, north of the site) sometime between 1970 and 1992, possibly in conjunction with the construction of the Vine Street Expressway or the subways.

Historic Streams

Based on a review of maps showing the locations of Philadelphia's historic streams, it appears that the former Pegg's Run (also known as Cohoquinoque Creek) may have been located nearby to the north of the site. The site also appears to be located near the headwaters of the former Dock Creek. See Appendix C for the approximate location of the former streams. Portions of the underground streams may be culverted.

Regional Geology

The site for the proposed development is located within the Lowland and Intermediate Upland Section of the Atlantic Coastal Plain Physiographic Province. Locally, the site is underlain by alluvial soils (Trenton Gravel Formation), as shown in the attached Geologic Formation Map, Figure 2. Based on our experience in this region of Philadelphia, bedrock at the site may consist of Oligoclase Mica Schist of the Wissahickon Formation. Pronounced, graduated weathered subsurface profiles are common in conjunction with the parent Wissahickon Mica Schist bedrock.

Subsurface Conditions

Based on our experience with similar project sites in Philadelphia, as well as information contained on the subway profiles, the subsurface conditions are expected to consist of fill / urban fill, underlain by alluvial soil, followed by the weathered remains (soil and rock) of the underlying bedrock formation. A more detailed description of the expected conditions is provided in the following sections:



Fill / Urban Fill

We anticipate fill material will be present across the site below the asphalt / concrete pavement and subbase materials, extending to varying depths depending on the historic original site grades and/or the depth of former basements. Urban fill material originates from the demolition of previous site buildings/features. As a result of the historic development / redevelopment at the site, the urban fill material at the site is expected to consist of a mixture of soil with varying amounts of building debris (brick, concrete, stone, wood, etc.). Remnants of former development features at the site may also be present, including foundations, slabs, basement walls, etc.

Alluvial Deposits

Alluvial soils are expected beneath the fill / urban fill layer at the site. The alluvial soils are expected to consist of layers of fine-grained (primarily at shallower depths) and coarse-grained soils. The alluvial soil layers are expected to be variable in terms of relative density.

Based on our experience in this region of Philadelphia, as well as the test boring data provided on the available subway cross-sections, we expect that the alluvial soils will extend to depths of about 30 feet to 40 feet below grade.

Decomposed Rock / Altered Rock

Beneath the alluvial deposits, we expect the subsurface conditions to transition gradually from decomposed rock to intact rock. This transition is often highly variable due to the uneven weathering of the bedrock formation.

Decomposed rock is a soil material that represents the partial weathering of the parent bedrock in that it retains some of its relic structure. Decomposed rock typically consists of relatively dense sand, with varying amounts of silt and gravel.

The decomposed rock typically transitions to altered rock with increased depth. Altered rock is the slightly weathered, very dense, typically friable material that retains the relic rock structure. The altered rock is differentiated from the decomposed rock based on the drilling and sampling response during advancement of test borings. Specifically, the altered rock layer refuses the split spoon sampler (i.e. more than 100 blow per 5 inches), but can be augered through with some difficulty. When cored, the altered rock typically has a Rock Quality Designation (RQD) of less than about 30.

Intact Rock

Intact rock is expected below the altered rock layer at depths of about 60 to 70 feet below grade. Due to the gradual weathering process of mica schist, the transition between altered rock and



intact rock is sometimes difficult to interpret during both drilling and construction. More durable quartz lenses may also be encountered which may give the impression that intact rock has been encountered. When cored, the intact rock typically has a RQD of greater than about 30.

Groundwater

We anticipate that the groundwater at the site will be encountered within the alluvial soil layer or decomposed rock stratum at estimated depths ranging from about 25 feet to 35 feet below grade. Seasonal fluctuations should be anticipated in the groundwater.

Development Considerations

The following is a summary of preliminary technical and practical development considerations based on this geotechnical engineering review.

- Subways – The presence of the subways may preclude developing certain areas of the site (due to limitations or access restrictions for developing within or above the respective rights-of-way). Development within less restricted areas of the site (i.e. the areas adjacent to the subways) should still account for the subways, particularly with respect to the foundations. Site features that are to be within the zone of influence of the subway must be designed to avoid impacting these structures. This applies to the design of permanent site features, as well as temporary uses (e.g. material lay down areas, hoists, crane pads, etc.). The presence of the subway may be prohibitive to tied-back temporary retention systems (internal bracing would likely be required). Deep foundations will likely be required to transmit loads below the subways, and to avoid imposing additional loads on the subway tunnels.
- Clean Fill – If the proposed development will result in a net export of soils and debris from the site, the exported materials will have to be tested to determine their compliance with PADEP Clean Fill criteria. The degree of testing is a function of the volume of soil to be removed from the site, presence of contamination, and the accepting facility's requirements. In our experience with sites located in an urban setting with a history of development such as at this site, we expect that some of the existing site soils will not meet the Clean Fill criteria, and will require additional testing and special disposal at approved facilities (e.g. as regulated waste). In addition, a Phase I ESA should be performed to help identify historic releases of controlled substances at the site, past cleanups, and potential underground storage tanks (USTs) that may remain at the site.
- Urban Fill – Urban fill material is expected at this site. The urban fill is typically not suitable for foundation support, and will therefore likely have to be removed and replaced in its entirety for foundation or slab-on-grade support. Alternatively, portions of the urban fill material may be able to be re-used on-site as structural fill; however, the material will require processing (i.e. crushing and screening) to remove deleterious



materials (metals, wood, etc.) and break the larger debris down to smaller fragments. The remnants of former development features such as foundations and slabs will probably have to be excavated and removed. If the historic structures were supported by deep foundations, this could result in challenges during construction in locations where the new foundation elements are proposed.

- Temporary Earth Retention Systems (TERS) – TERS will be required for construction of below-grade excavations such as basements or other features that extend to the perimeter edges of the site, and where the excavations cannot be safely benched or stepped. A variety of TERS systems are available, depending on the subsurface conditions encountered, as well as the depth and location of the proposed excavation. Alternative TERS systems include soldier pile and timber lagging, concrete secant walls, and other internally braced excavations. As noted above, TERS near the subway tunnels will likely have to be internally braced.
- Historic Existing Streams – Streams may have once flowed in the vicinity of the project site. The presence of an underground stream can create unique development challenges, particularly with respect to the use of conventional spread footing foundations, general subgrade preparation, and groundwater control. Test borings will be necessary to confirm the presence or absence of the stream bed underlying the site, and to understand the impact on the proposed development.
- Groundwater – Depending on the extent of proposed below-grade features for the development, consideration for management of groundwater during and after construction will be required. If basement levels are expected to extend near or below the groundwater table, perimeter basement and underslab drainage systems, damp-proofing, and/or waterproofing will be necessary. Groundwater management during construction may require the use of conventional sumps and pumps, or more aggressive permanent well points. Dewatering at the project site, if necessary, should be designed to consider possible impacts on the neighboring site features.
- Foundations – Based on the expected subsurface conditions at the site, we anticipate that conventional foundation systems used in the City of Philadelphia will be appropriate for this site. Lightly loaded low-rise structures can likely be supported by conventional spread footings, possibly with some form of ground improvement (aggregate piers, etc.) if poor subgrade conditions exist. More heavily loaded mid- to high-rise structures (as well as structures located adjacent to the existing subways) will likely require deep foundations for support. Alternative deep foundation systems include drilled shafts, augered pressure-grouted piles, and micro-piles. Foundation systems and ground improvement techniques that impart extreme vibrations are generally not recommended due to possible disturbance to adjacent site features (buildings, subway, underground utilities, etc.), particularly without some form of vibration and existing feature distress monitoring program.



Conclusions

With the exception of the existing subways, which may restrict the development opportunities in certain portions of the site, we have not identified unusual conditions that would preclude the development of this project site. The development considerations outlined above are common for this region of Philadelphia.

Our evaluations are based on research of readily available information regarding the site history, geology, and the nearby subway lines. We recommend that a thorough geotechnical exploration be performed to better understand the subsurface conditions at the site, and their impact on the proposed development.

We thank you for the opportunity to assist you in this preliminary due diligence phase of the project. Should you have any questions or require any additional information, please do not hesitate to contact us.

Very truly yours,

MASER CONSULTING P.A.

A handwritten signature in black ink, appearing to read 'Philip E. Gauffreau'.

Philip E. Gauffreau, P.E.
Discipline Leader – Geotechnical Services

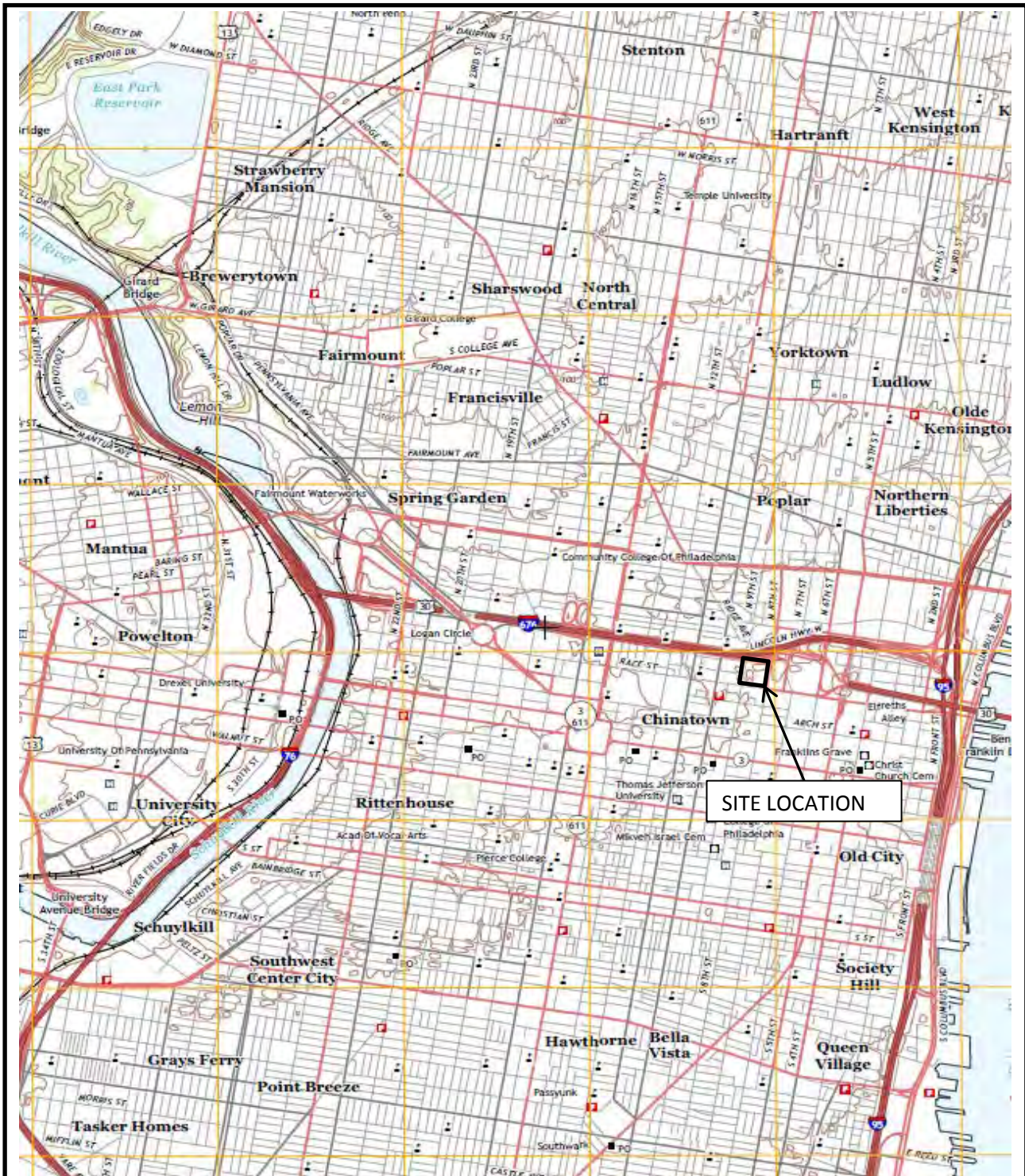
A handwritten signature in blue ink, appearing to read 'Michael J. Kwiatkowski'.

Michael J. Kwiatkowski
Senior Project Manager

PEG/MJK/djl
Attachments (Figure 1, 2, and 3; Appendix A, B, and C)
cc: R. Chiliberti, Maser Consulting

r:\alloffices\philadelphia\projects\2015\15001540a\geotechnical documents\geo report documents\160901_peg_labrum - 800 vine.docx

FIGURE 1
Site Location Map



NOTES:

1.) *SITE MAP OBTAINED FROM USGS TOPOGRAPHIC MAP, PHILADELPHIA, PA-NJ QUADRANGLE, DATED 2013.



Consulting, Municipal & Environmental Engineers
Planners ■ Surveyors ■ Landscape Architects

New Jersey New York Pennsylvania Virginia
Customer Loyalty through Client Satisfaction

Title: SITE LOCATION MAP		
Project: PROPOSED REDEVELOPMENT 800-30 VINE STREET PHILADELPHIA, PA		
Drawn By:	MJIK	Checked By: PEG
Scale:	N.T.S.	Date: 8/29/2016
Project No.:	15001540A	Figure No.: 1

FIGURE 2
Geologic Map



NOTES:

- 1.) *BASE MAP OBTAINED FROM PENNSYLVANIA DEPARTMENT OF NATURAL RESOURCES.
<http://www.gis.dcnr.state.pa.us/maps/>

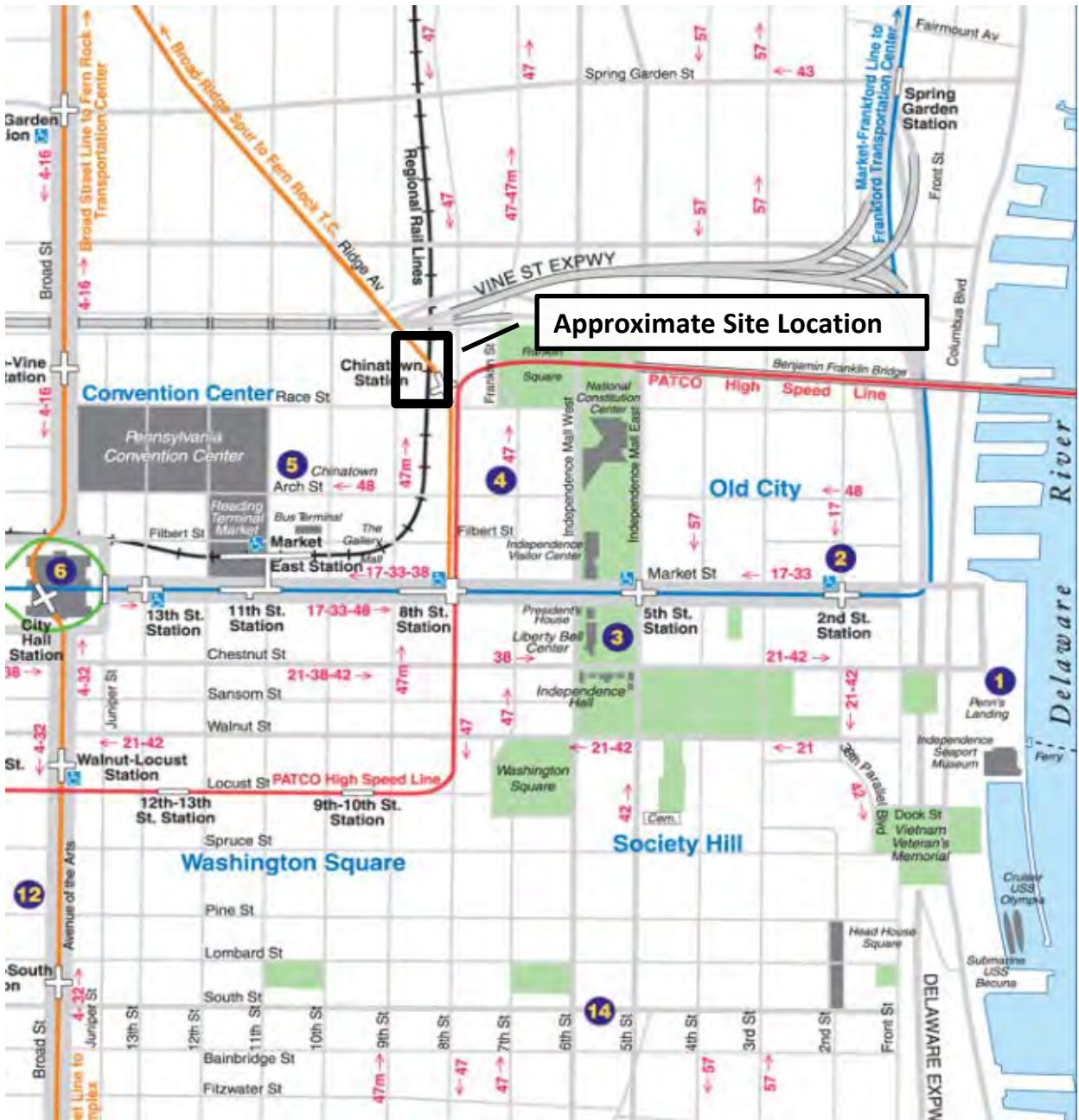


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 CONSULTING P.A.
 Consulting, Municipal & Environmental Engineers
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New Jersey New York Pennsylvania Virginia
 Customer Loyalty through Client Satisfaction

Title: GEOLOGIC FORMATION MAP		
Project: PROPOSED DEVELOPMENT 800-30 VINE STREET PHILADELPHIA, PA		
Drawn By:	MJK	Checked By: PEG
Scale:	N.T.S.	Date: 8/29/2016
		Project No.: 15001540A
		Figure No.: 2

FIGURE 3
Transit Map



NOTES:

1.) *BASE MAP OBTAINED FROM "CENTER CITY PHILADELPHIA STREET & SEPTA TRANSIT MAP," <http://www.visitphilly.com/>



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New Jersey New York Pennsylvania Virginia
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Title:			TRANSIT MAP		
Project:			PROPOSED DEVELOPMENT 800-30 VINE STREET PHILADELPHIA, PA		
Drawn By:	MJIK	Checked By:	PEG	Project No.:	15001540A
Scale:	N.T.S.	Date:	8/29/2016	Figure No.:	3

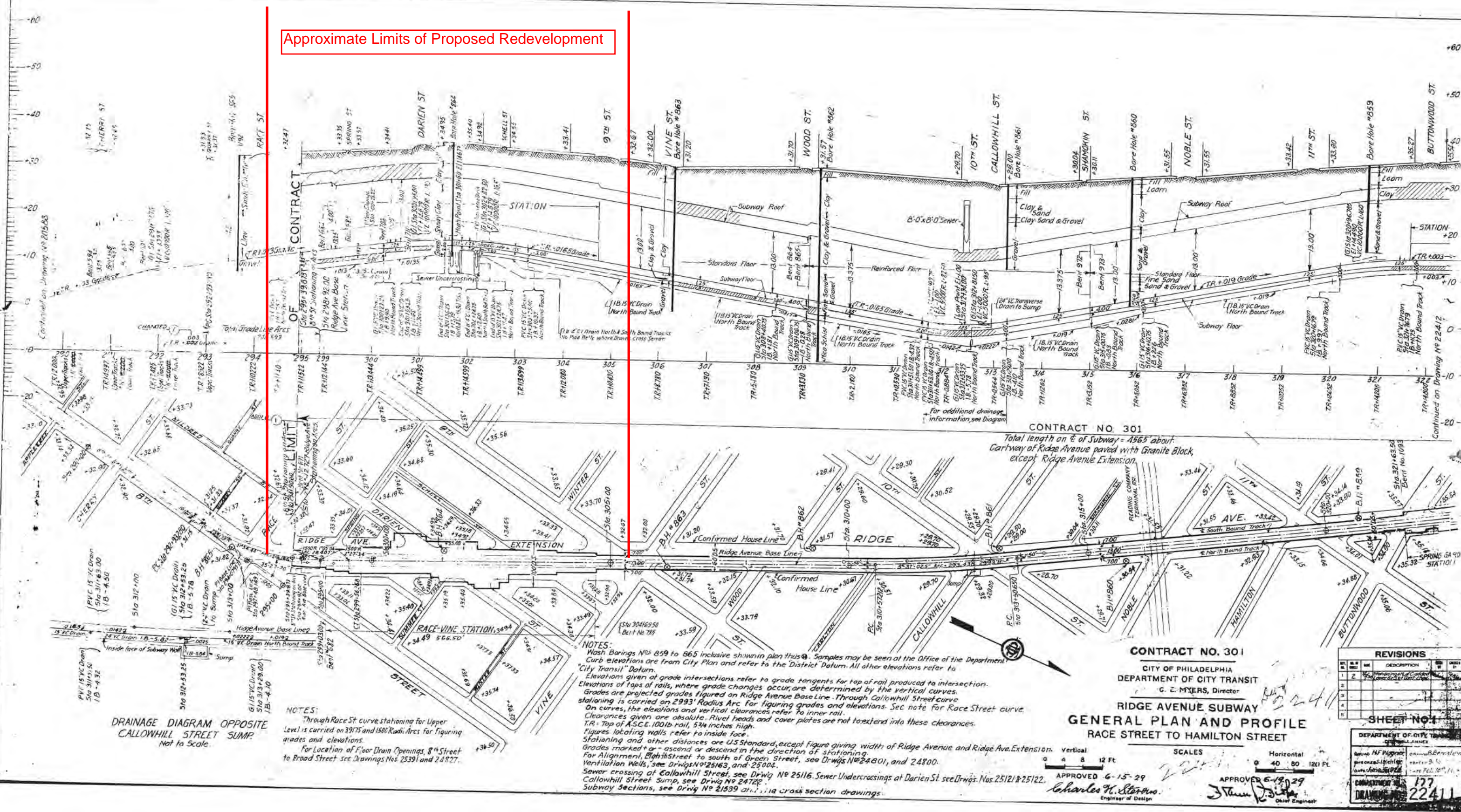
APPENDIX A

“General Plan and Profile”

Contract No. 301 – Ridge Avenue Subway (Race St. to Hamilton St.)

Contract No. 302 – Eighth Street Subway (Walnut St. to Race St.)

Approximate Limits of Proposed Redevelopment



NOTES:
 Wash Borings Nos 859 to 865 inclusive shown in plan this. Samples may be seen at the Office of the Department.
 Curb elevations are from City Plan and refer to the District Datum. All other elevations refer to City Transit Datum.
 Elevations given of grade intersections refer to grade tangents for top of rail produced to intersection.
 Elevations of tops of rails, where grade changes occur, are determined by the vertical curves.
 Grades are projected grades figured on Ridge Avenue Base Line. Through Callowhill Street curve stationing is carried on 2993' Radius Arc for figuring grades and elevations. See note for Race Street curve.
 On curves, the elevations and vertical clearances refer to inner rail.
 Clearances given are absolute. Rivet heads and cover plates are not to extend into these clearances.
 T.R.: Top of ASCE 100lb rail, 5 3/4 inches high.
 Figures locating walls refer to inside face.
 Stationing and other distances are U.S. Standard, except figure giving width of Ridge Avenue and Ridge Ave. Extension.
 Grades marked + or - ascend or descend in the direction of stationing.
 For Alignment, Eighth Street to south of Green Street, see Drwg's No 24801, and 24800.
 Ventilation Wells, see Drwg's No 25163, and 25004.
 Sewer crossing at Callowhill Street, see Drwg No 25116. Sewer Undercrossings at Darien St. see Drwg's Nos. 25121 & 25122.
 Callowhill Street Sump, see Drwg No 24722.
 Subway Sections, see Drwg No 21839 and its cross section drawings.

CONTRACT NO. 301
 Total length on E of Subway = 4565' about
 Carway of Ridge Avenue paved with Granite Block, except Ridge Avenue Extension.

CONTRACT NO. 301
 CITY OF PHILADELPHIA
 DEPARTMENT OF CITY TRANSIT
 C. Z. MYERS, Director
RIDGE AVENUE SUBWAY
GENERAL PLAN AND PROFILE
RACE STREET TO HAMILTON STREET

Vertical SCALES 0 4 8 12 Ft
 Horizontal SCALES 0 40 80 120 Ft
 APPROVED 6-15-29
 Charles H. Stearns, Engineer of Design
 APPROVED 6-19-29
 John J. Dittus, Chief Engineer

REVISIONS			
NO.	DATE	DESCRIPTION	BY
1			
2			
3			
4			

SHEET NO. 1
 DEPARTMENT OF CITY TRANSIT
 PHILADELPHIA
 DRAWING NO. 22411

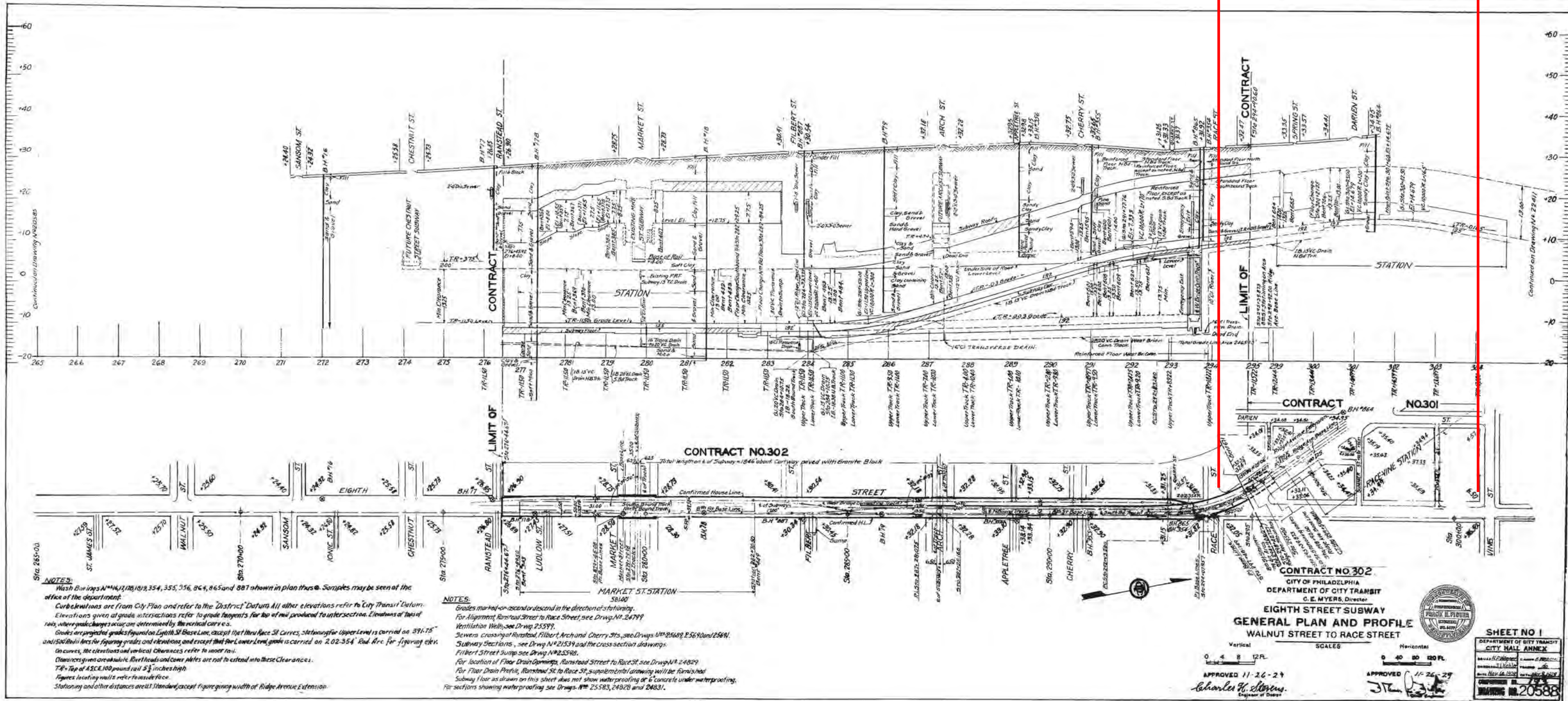
Continued on Drawing No 20563

Continued on Drawing No 22412

DRAINAGE DIAGRAM OPPOSITE
ALLOWHILL STREET SUMP
 Not to Scale.

NOTES:
 Through Race St curve stationing for Upper Level is carried on 39175 and 1806 Radii Arcs for figuring grades and elevations.
 For Location of Floor Drain Openings, 8th Street to Broad Street see Drawings Nos 25391 and 24827.

Approximate Limits of Proposed Redevelopment



NOTES:
 Wash Borings N^{os} 46, 47, 48, 10, 9, 354, 355, 356, 864, 865 and 887 shown in plan thus. Samples may be seen at the office of the department.
 Curbs and elevations are from City Plan and refer to the District Datum. All other elevations refer to City Transit Datum.
 Elevations given at grade intersections refer to grade fragments for top of rail produced to intersection. Elevations of these rails, where grade changes occur, are determined by the vertical curves.
 Grades are projected grades figured on Eighth St Base Line, except that three Race St Curves, stationing for upper level is carried on 391.75' and 1500 Rail lines for figuring grades and elevations and except that for Lower Level, grade is carried on 2.02-354' Rad. Arc for figuring elev.
 On curves, the elevations and vertical clearances refer to inner rail.
 Clearances given are absolute. River heads and cover pipes are not to extend into these clearances.
 TR - Top of A3CE, 100 pound rail 5 1/2 inches high.
 Figures locating walls refer to inside face.
 Stationing and other distances are U.S. Standard, except figuring width of Ridge Avenue Extension.

NOTES:
 Grades marked as ascend or descend in the direction of stationing.
 For Alignment, Runstead Street to Race Street, see Drwg. No. 24799.
 Ventilation Wells, see Drwg. 25599.
 Sewers Crossing Runstead, Filbert, Arch and Cherry Sts., see Drwgs. N^{os} 25689, 25690 and 25691.
 Subway Sections, see Drwg. N^o 21539 and the cross section drawings.
 Filbert Street Sump, see Drwg. N^o 25598.
 For location of Floor Drain Openings, Runstead Street to Race St. see Drwg. N^o 24829.
 For Floor Drain Profiles, Runstead St. to Race St., supplementary drawings will be furnished.
 Subway floor as drawn on this sheet does not show waterproofing of concrete under waterproofing.
 For sections showing waterproofing see Drwgs. N^{os} 25583, 24828 and 24831.

CONTRACT NO. 302
 CITY OF PHILADELPHIA
 DEPARTMENT OF CITY TRANSIT
 C. E. MYERS, Director

CONTRACT NO. 301
 CITY OF PHILADELPHIA
 DEPARTMENT OF CITY TRANSIT
 C. E. MYERS, Director

EIGHTH STREET SUBWAY
 GENERAL PLAN AND PROFILE
 WALNUT STREET TO RACE STREET

Scale: 1" = 20' Vertical, 1" = 100' Horizontal

APPROVED 11-26-24
 Charles H. Stevens, Engineer in Charge

APPROVED 11-26-29
 J. H. [Signature]

SHEET NO. 1
 DEPARTMENT OF CITY TRANSIT
 CITY HALL ANNEX
 DRAWING NO. 20588

APPENDIX B
Historic Streams

Historic Streams of Philadelphia

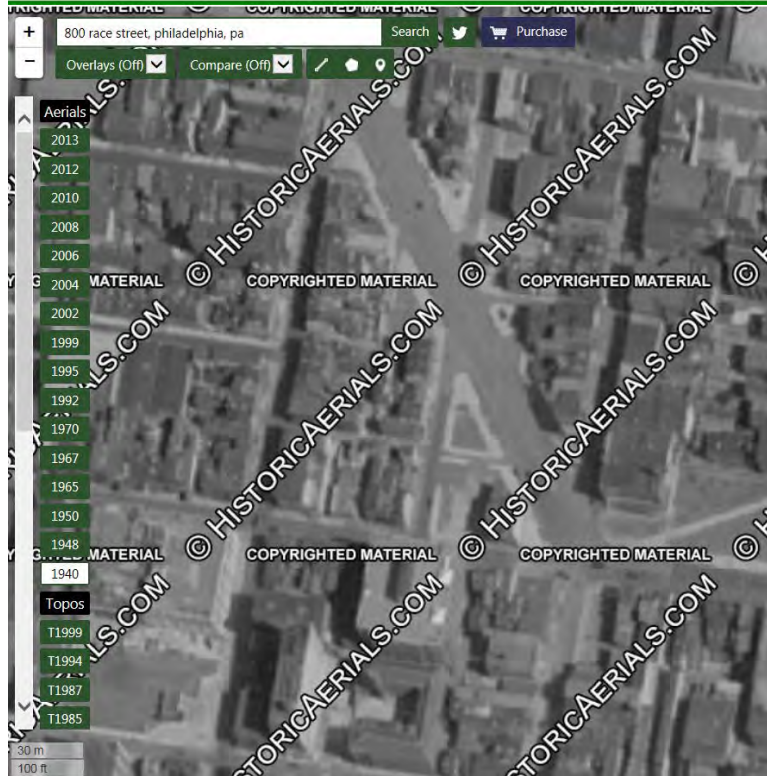
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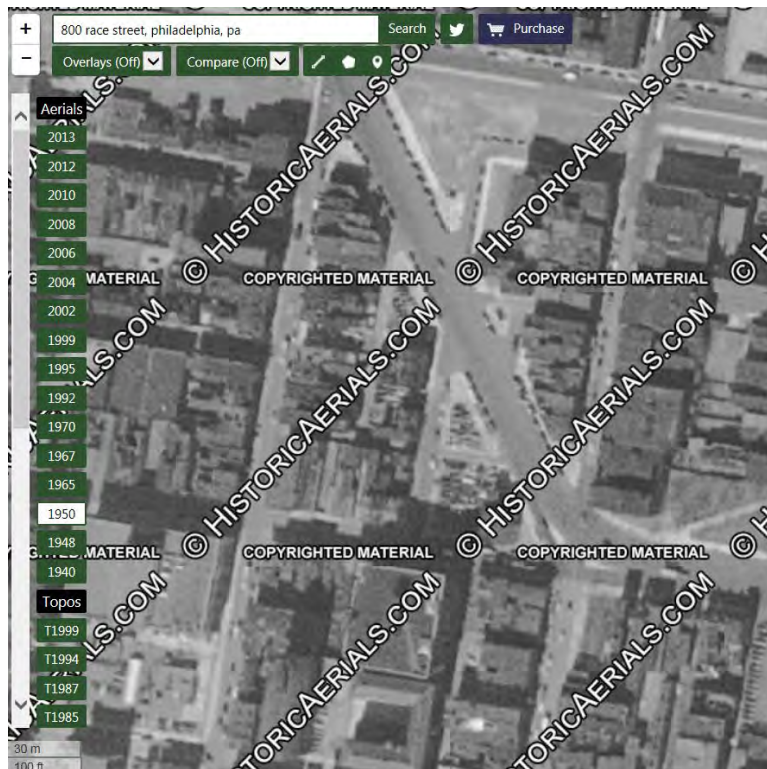
APPENDIX C
Historic Aerial Images

Historic Aerial Imagery – 800-30 Vine Street, Philadelphia, PA
www.historicaerials.com

1940

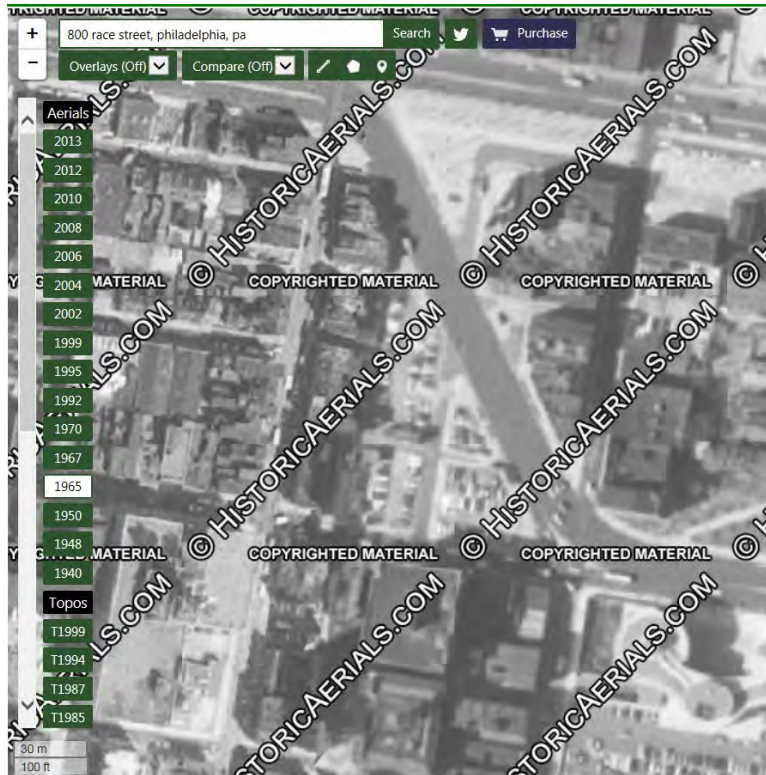


1950

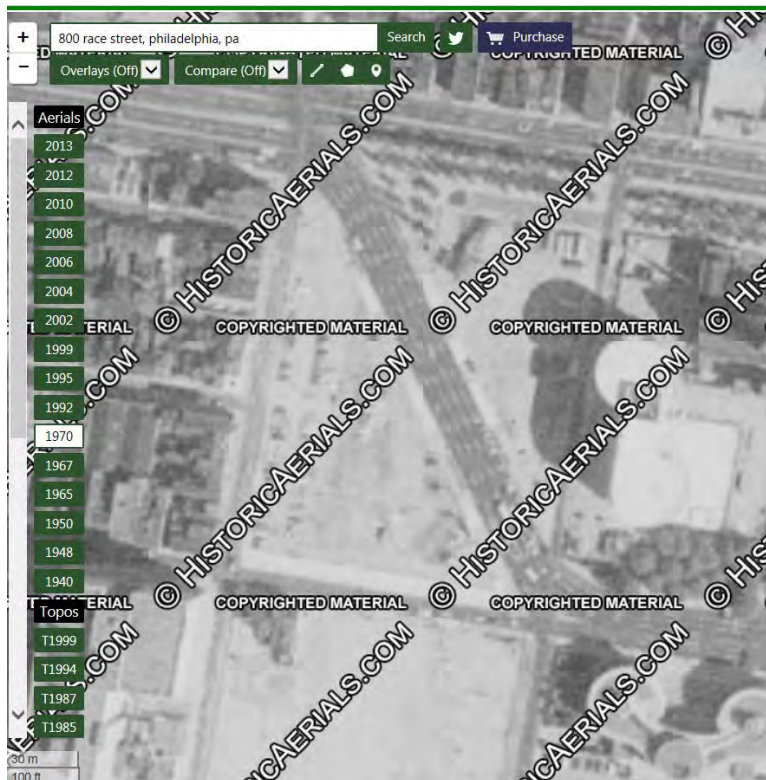


Historic Aerial Imagery – 800-30 Vine Street, Philadelphia, PA
www.historicaerials.com

1965

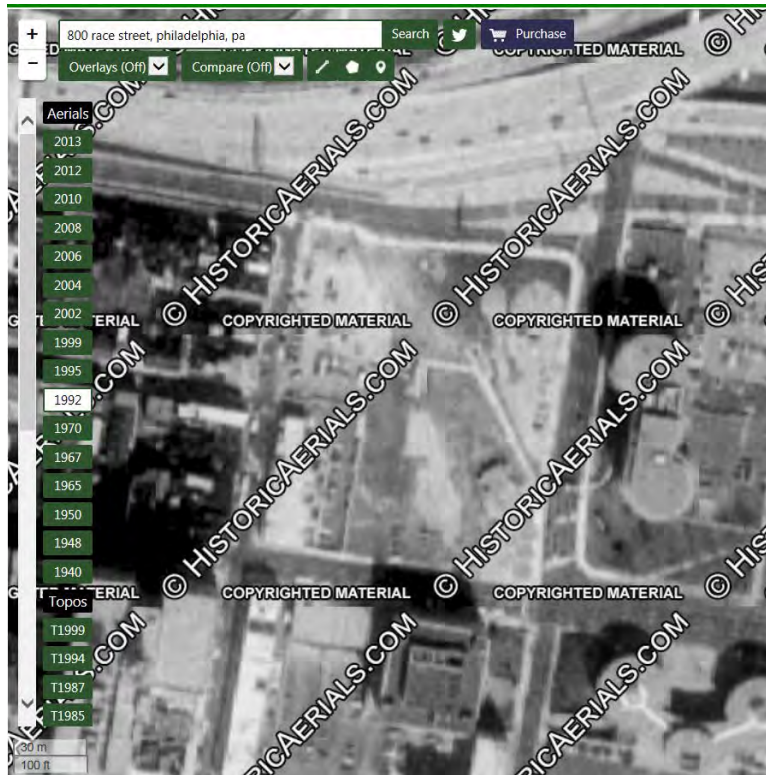


1970

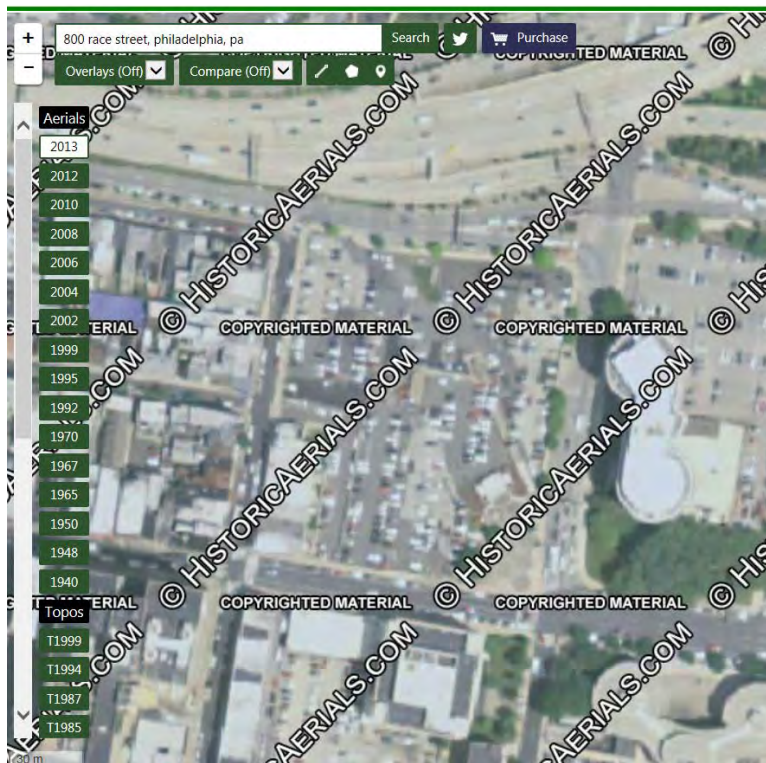


Historic Aerial Imagery – 800-30 Vine Street, Philadelphia, PA
www.historicaerials.com

1992



2013





Engineers
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Landscape Architects
Environmental Scientists

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September 1, 2016
VIA E-MAIL

Mr. Robert LaBrum
Philadelphia Redevelopment Authority
1234 Market Street, 16th Floor
Philadelphia, PA 19107

**Re: 800-30 Vine Street
City of Philadelphia, PA
Project No. 15001540A**

Dear Robert:

Regarding the above referenced project, Attached please find the following items:

- One (1) copy of the Development Limit Plan, dated 9/1/16.
- One (1) copy of the Boundary and Topographic survey prepared by our office, dated 4/29/16.
- One (1) copy of the FEMA floodplain map.

Below please find our preliminary review of the subject property which is based on the following documents:

- City of Philadelphia Zoning Ordinance
- Boundary and Topographic survey prepared by our office, dated 4/29/16
- FEMA floodplain map

Boundary and Topographic Survey Review:

- Multiple existing utilities are identified within the development limit area. It is recommended that a meeting is coordinated with each utility company to confirm if the utilities can be removed and/or relocated.
- Multiple existing restrictions are noted on the plans (items 1 through 9): Meetings should be setup with each department in the city to discuss the potential to construct a structure over the existing restriction, to maximize the use of the property.
- It is highly recommended the PRA order a title report to confirm the restrictions noted on the plans (items 1 through 9).
- The below grade subway limits should be confirmed to determine the complete development limits.
- The existing topography is generally flat which is positive for land development.

- The site is located outside of the 500 year floodplain which is positive for land development.

Zoning Map (Ch. 14-400):

Zoning District: The site base zoning district is CMX-4, Center City Commercial Mixed-Use District. (Refer to the Appendix for the zoning map).

Overlay Districts (Ch. 14-500): The site is located within three (3) overlay districts which includes a height restriction for the Center City Overlay:

- CTR Center City Overlay District – Center City Residential District Control Area:
Does not affect CMX-4
- CTR Center City Overlay District – Center City Commercial District Control Area:
Identifies supplemental uses within section 14-502(5) which is in addition to the base zoning district.
- CTR Center City Overlay District – Vine Street Area (14-502(7)(e))
 - **Includes signage regulations.** Refer to the appendix of this report for the requirements.
 - **All development within an overlay district must comply with Ch. 14-1000 (Historic Preservation) as applicable – Historic due diligence will be required to confirm.**
- City Hall View Corridor Northeast 2: **The maximum height increases from 330 ft. along a 45 degree recession plane from the outer edge of Northeast View Corridor 1 to the outer edge of Northeast View Corridor 2, such that the maximum height at the outer edge of Northeast View Corridor 2 is 490 ft. (Refer to the appendix for the exhibit detail).**

We recommend meeting with City Planning and L&I to confirm this requirement applies to our site, as the corridor is adjacent to our property.

Yard Definition(14-701(1.)(d)(2.): the property is “bounded by three or more streets” therefore, all yards abutting a street are front yards and the remaining lot line shall be considered a rear yard. The side yard requirements of the zoning district shall not apply.

Commercial Districts: Multiple principle uses and buildings are allowed on a single lot in commercial districts.

Permitted Uses (14-600):

- CMX-4 Zoning District: Refer to the Appendix for the Use Table.
- CTR Center City Overlay District – Center City Commercial District Control Area: Identifies supplemental uses within section 14-502(5) which is in addition to the base zoning district.

General:

Setbacks for Zone Blocks with More than One Zone: NOT APPLICABLE

Split-zoned properties: NOT APPLICABLE

Development Standards (Bulk Requirements – 14-700)

CMX-4 Zoning District Bulk Requirements (14-701-3):

1. Minimum District Area: n/a
2. Minimum Lot Width: n/a
3. Minimum Lot Frontage: n/a
4. Minimum Lot Area: n/a
5. Minimum Open Area: Buildings \leq 5 stories with 1 or more dwelling units: 10%; Others: 0%*
6. Minimum Lot Depth: n/a
7. Max. Floor Area: 500%*
8. Max. Occupied Area: Buildings \leq 5 stories with 1 or more dwelling units: 90; Others: 100
9. Max. Building Height: Subject to Sky Plane requirements and overlay requirements below*
10. Setbacks:
 - a. Minimum Front Yard Setback: n/a
 - b. Minimum Rear Yard Setback: n/a
 - c. Minimum Side Yard Setback: If used: Buildings \leq 4 stories with three or fewer dwelling units = 5; Others = 8

***CMX-4 and CMX-5 Bulk and Massing Controls (14-701(5)):** Refer to the appendix of this report for the requirements.

***City Hall View Corridor Northeast 2:** The maximum height increases from 330 ft. along a 45 degree recession plane from the outer edge of Northeast View Corridor 1 to the outer edge of Northeast View Corridor 2, such that the maximum height at the outer edge of Northeast View Corridor 2 is 490 ft. (Refer to the appendix for the exhibit detail).

Exceptions to dimensional standards (14-701(6)): Refer to the Appendix for this information

Floor Area and Height Bonuses (14-702): Refer to the appendix of this report for the requirements.

Form and Design (14-703)(Architectural Requirements): Refer to the appendix of this report for the requirements.

Trash and Recycling Storage Areas (14-604(12)):

The following standards apply to accessory trash and recycling storage areas in all Commercial districts:

- (a) The total area of trash and recycling storage areas may not exceed 25% of the gross floor area in the principal structure.
- (b) The maximum height regulations of the subject zoning district apply to accessory trash and recycling storage areas.

Parking and Loading (14-800):

1. Parking: Refer to the Appendix for required Parking in CMX-4 (based on the proposed uses)
 - a. Parking reductions may apply (14-802(8)) (refer to appendix)
2. Parking Dimensions:
 - a. 90 degrees: 8.5 ft x 18 ft with 24ft drive aisle
 - b. Compact Spaces for lots with more than 25 spaces: 8 ft x 16 ft (up to a maximum of 25% of the parking lot)
3. ADA accessible parking spaces(14-802(5)): Refer to the appendix.
4. Preferential Parking:

Buildings with a principal use in the public, civic, or institutional, office, retail sales, commercial services, or vehicle and vehicular equipment sales and service use categories with 30 or more parking spaces shall designate at least 5% of the parking spaces for carpool, vanpool parking, and hybrid/alternative fuel vehicles. These preferential parking spaces shall be located closer to the principal building than other parking with the exception of accessible parking.
5. Parking spaces are prohibited in the front, side, and rear yards.
6. Surface parking and garages require a special exception approval for parking located in the CMX-4 zoning district (Granted by the ZBA).
7. Minimum drive aisle and driveways area: 25% of the parking lot.
8. Parking Design Standards (14-803-4) – Refer to the Appendix for the design criteria
9. Maximum curb cut width: 24ft
10. Loading: CMX-4: Required – Refer to the Appendix for the design criteria
11. Reservoir Space (14-803(2)(c)): Refer to the Appendix for the design criteria
12. Electric vehicle parking spaces (14-803(6)): Refer to the Appendix for the design criteria
13. Bicycle Parking (14-804): Refer to the Appendix for bicycle parking criteria

Conclusion:

Based on the above information, the areas identified as developable areas on the attached plan are feasible for development assuming the following due diligence items are completed:

- Order a title report to confirm the restrictions shown on the plan.
- Confirm if the existing utilities can be removed and/or relocated within the developable areas.
- Subsurface investigation of the subway limits.
- Identify the limits of construction adjacent to the subway(must be coordinated with SEPTA)
- Confirm if the site has a historic designation.
- Schedule a meeting with City Planning and L&I to confirm the City Hall View Corridor Northeast 2 requirement applies to our site.

Further development within the restriction areas (whether a structure, open-air parking, etc.) must be confirmed by coordinating meetings with each agency associated with each restriction.

Please note, the type of development of the subject property (commercial, residential, project density, etc.) is highly subject to City and State approvals including further engineering and due diligence (ie. environmental phase 1 report, geotechnical investigation, sub-surface utility investigation, etc.)

If you should have any questions or require additional information, please don't hesitate to contact our office.

Very truly yours,

MASER CONSULTING P.A.



Rhett N. Chiliberti, PE
Geographic Discipline Leader



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APPENDIX

- ❖ **CMX-4 USE TABLE**
- ❖ **CMX-4 BULK AND MASSING REQUIREMENTS (SKY PLANE)**
- ❖ **FLOOR AREA AND HEIGHT BONUSES**
- ❖ **FORM AND DESIGN**
- ❖ **CMX-4 PARKING REQUIREMENTS**
- ❖ **CTR CENTER CITY OVERLAY DISTRICT – VINE STREET AREA REQUIREMENTS**
- ❖ **PARKING DESIGN STANDARDS**
- ❖ **PARKING REDUCTIONS**
- ❖ **BICYCLE PARKING**
- ❖ **LOADING**
- ❖ **EXCEPTIONS TO DIMENSIONAL STANDARDS**
- ❖ **CITY HALL VIEW CORRIDOR NORTHEAST 2**
- ❖ **ZONING MAP**

Single-Family	[1]	Y[3]	Y[4]	N	N	N	N	N	
Two-Family	[1]	Y[3]	Y[4]	Y[7]	N	N	N	N	
Multi-Family	[1][2]	Y[2][3]	Y[2][4]	Y	Y	Y	N	N	
Group Living (except as noted below)	N	N	N	Y	Y	Y	N	N	
Personal Care Home	N	S	S[4]	Y	Y	Y	Y	N	§ 14-603(11)
Single-Room Residence	N	N	N	Y	Y	Y	N	N	
Parks and Open Space Use Category									
Passive Recreation	Y	Y	Y	Y	Y	Y	Y	Y	
Active Recreation	S	S	S	Y	Y	Y	Y	Y	
Public, Civic, and Institutional Use Category									
Day Care (as noted below)									
Family Day Care	Y	Y	Y	Y	Y	Y	N	N	§ 14-603(5)
Group Day Care	Y	Y	Y	Y	Y	Y	Y	Y	§ 14-603(5)
Day Care Center	S	Y	Y	Y	Y	Y	Y	Y	§ 14-603(5)
Educational Facilities	N	Y	N	Y	Y	Y	N	Y	
Fraternal Organization	N	Y	S	Y	Y	Y	N	Y	
Hospital	N	Y	Y	Y	Y	Y	N	Y	
Libraries and Cultural Exhibits	Y	Y	Y	Y	Y	Y	Y	Y	
Religious Assembly	Y	Y	S	Y	Y	Y	N	Y	
Safety Services	Y	Y	Y	Y	Y	Y	Y	Y	
Transit Station	Y	S	S	Y	Y	Y	S	Y	
Utilities and Services, Basic	S	S	N	S	S	S	S	S	
Wireless Service Facility (as noted below)									
Freestanding Tower	S	S	S	Y	Y	Y	S	Y	§ 14-603(16)
Building or Tower-Mounted Antenna	Y	Y	Y	Y	Y	Y	Y	Y	§ 14-603(17)
Office Use Category									
Business and Professional	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Medical, Dental, Health Practitioner (as noted below)									
Sole Practitioner	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Group Practitioner	S[5]	S	S	Y	Y	Y	Y	Y	
Government	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Retail Sales Use Category									
Building Supplies and Equipment	Y[5]	Y	Y	Y	Y	Y	Y	Y	§ 14-603(3)
Consumer Goods (except as noted below)	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Drug Paraphernalia Sales	N	N	N	N	N	N	N	N	§ 14-603(13)
Gun Shop	N	N	N	N	N	N	N	N	§ 14-603(13)
Food, Beverages, and Groceries	Y[5]	Y	Y	Y	Y	Y	Y	Y	§ 14-603(7)
Pets and Pet Supplies	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Sundries, Pharmaceuticals, and Convenience Sales	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Wearing Apparel and Accessories	Y[5]	Y	Y	Y	Y	Y	Y	Y	

Previous District Name	C-1	C-2/RC-2	(/NCC)	C-3/RC-3	C-4	C-5	C-7/NSC	ASC	
District Name	CMX-1	CMX-2	CMX-2.5	CMX-3	CMX-4	CMX-5	CA-1	CA-2	Use-Specific Standards
<p>Y = Yes permitted as of right S = Special exception approval required N = Not allowed (expressly prohibited) Uses not listed in this table are prohibited See § 14-602(4)(a) (Notes for Table 14-602-2) for information pertaining to bracketed numbers (e.g., “[2]”) in table cells.</p>									
Commercial Services Use Category									
Animal Services (except as noted below)	Y[5]	S	S	Y	Y	Y	Y	Y	
Boarding and Other Services	N	N	N	N	N	N	N	N	§ 14-603(14)
Assembly and Entertainment (except as noted below)	N	S	S	S	Y	Y	S	Y	
Amusement Arcade	N	N	N	N	N	N	N	N	§ 14-603(13)
Casino	N	N	N	N	N	N	N	N	
Nightclubs and Private Clubs	N	S	N	S	Y	Y	N	Y	§ 14-603(18)
Pool or Billiards Room	N	N	N	N	N	N	N	N	§ 14-603(13)
Building Services	N	N	N	Y	Y	Y	N	Y	
Business Support	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Eating and Drinking Establishments (as noted below)									
Prepared Food Shop	S[5]	Y	Y	Y	Y	Y	Y	Y	
Take-Out Restaurant	N	S	S	Y	Y	Y	S	Y	§ 14-603(6)
Sit Down Restaurant	N	Y	Y	Y	Y	Y	Y	Y	
Financial Services (except as noted below)	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Personal Credit Establishment	N	N	N	N	N	N	N	N	§ 14-603(13)
Funeral and Mortuary Services	S[5]	Y	N	Y	Y	Y	Y	Y	
Maintenance and Repair of Consumer Goods (except as noted below)	Y[5]	Y	Y	Y	Y	Y	Y	Y	
On-Premise Dry Cleaning	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Marina	N	N	N	Y	Y	Y	N	N	
Parking, Non-Accessory (as noted below)									
Surface Parking	N	S	N	S	N	N	S	Y	§ 14-603(10)
Structured Parking	N	S	S	Y	[6]	[6]	S	Y	§ 14-603(10)
Personal Services (except as noted below)	Y[5]	Y	Y	Y	Y	Y	Y	Y	
Body Art Service	N	N	N	N	N	N	N	N	§ 14-603(2); § 14-603(13)
Fortune Telling Service	N	N	N	Y	Y	Y	Y	Y	
Radio, Television, and Recording Services	N	Y	Y	Y	Y	Y	Y	Y	
Visitor Accommodations	N	N	N	Y	Y	Y	N	Y	
Commissaries and Catering Services	N	Y	Y	Y	Y	Y	Y	Y	
Vehicle and Vehicular Equipment Sales and Services Use Category									
Commercial Vehicle Sales and Rental	N	N	N	N	N	N	N	S	
Personal Vehicle Repair and Maintenance	N	N	N	Y	Y	Y	N	N	
Personal Vehicle Sales and Rental	N	N	N	Y	Y	Y	N	S	
Vehicle Fueling Station	N	N	N	S	N	N	S	Y	§ 14-603(8)
Vehicle Equipment and Supplies Sales and Rental	N	Y	N	Y	Y	Y	S	S	
Wholesale, Distribution, and Storage Use Category									
Moving and Storage Facilities	N	N	N	N	N	N	N	Y	
Wholesale Sales and Distribution	N	N	N	N	N	N	N	Y	§ 14-603(1)
Industrial Use Category									

Artist Studios and Artisan Industrial	N	Y	Y	Y	Y	Y	N	Y	
Research and Development	N	Y	Y	Y	Y	Y	Y	Y	
Urban Agriculture Use Category									
Community Garden	Y	Y	Y	Y	Y	Y	Y	Y	§ 14-603(15)
Market or Community-Supported Farm	Y	Y	Y	Y	N	N	Y	Y	§ 14-603(15)

CMX-4 Bulk and Massing Requirements:

(5) CMX-4 and CMX-5 Bulk and Massing Controls.

(a) Applicability.

(.1) Buildings on CMX-4 and CMX-5 lots shall demonstrate compliance with this § [14-701\(5\)](#) (CMX-4 and CMX-5 Bulk and Massing Controls) using either Option A (Sky Plane Controls), as provided in § [14-701\(5\)\(b\)](#), or Option B (Open Area, Building Width, Spacing, and Height Controls), as provided in § [14-701\(5\)\(c\)](#).

(.2) If a building is subject to the provisions of both § [14-701\(5\)\(b\)](#) and § [14-701\(5\)\(c\)](#), the applicant may choose to demonstrate compliance in accordance with either option.

(.3) If a building is subject to the provisions of § [14-701\(5\)\(c\)](#) only, then the building shall comply with § [14-701\(5\)\(c\)](#).

(.4) When the bulk and massing standards of this § [14-701\(5\)](#) conflict with the floor area ratio standards of § [14-701\(3\)](#) (Commercial Districts Dimensional Table), the stricter standard shall govern.

(b) Option A: Sky Plane Controls.

The bulk and massing provisions of this § [14-701\(5\)\(b\)](#) apply to any lot frontage facing a street 50 ft. or more in width, except for lots that are located across a street from a public park and where the distance from the lot frontage to the furthest boundary of the public park is greater than 200 ft. The street width shall be the width of the right-of-way as confirmed on the City Plan of streets.

(.1) Sky Plane Standards.

(.a) For the purposes of this § [14-701\(5\)\(b\)](#) (Option A: Sky Plane Controls), the term "sky plane" shall mean the set of building bulk and massing that allows visibility of the sky from sidewalk level.

(.b) Buildings may occupy 100% of the lot area below the height threshold for that street as shown in Column B of [Table 14-701-5](#).

(.c) Any portion of a building above the height thresholds listed in Column B of [Table 14-701-5](#) is subject to form controls that limit its massing and bulk in relation to its context so that specific amounts of sky visibility are retained.

(.d) Within each interval of building height shown in Column C of [Table 14-701-5](#), the building may not block more than the percentage of sky plane shown in Column D of [Table 14-701-5](#).

(.e) Sky plane is calculated in relation to a viewing line located at grade at the center of the relevant street and a vertical plotting plane located at the building line. The regulations of the

Commission shall contain the method for calculating percentage block of the sky plane, as well as detailed information on materials to be submitted to demonstrate compliance with sky plane requirements (which shall include a signed and sealed statement from a licensed architect certifying compliance with sky plane requirements).

(.f) If two or more building frontages are subject to the provisions of this § [14-701\(5\)](#) (CMX-4 and CMX-5 Bulk and Massing Controls), then the sky plane limits in [Table 14-701-5](#) of the widest street fronting the lot shall apply to each facade. For example, if a lot has frontage on one street that is 70 feet wide and a second street that is 60 feet wide, both frontages must comply with the sky plane requirements applicable for a street 70 feet wide.

(.g) If three or more building frontages are subject to the provisions of this § [14-701\(5\)](#) (CMX-4 and CMX-5 Bulk and Massing Controls) then as an alternative to subsection (.d), above, the building's average of sky plane blockage at each regulated height level of all frontages subject to the sky plane provisions may be used to demonstrate compliance with the allowed blockage of sky plane in Column D of [Table 14-701-5](#).³⁸¹

(.h) L&I shall not issue a zoning permit unless the Commission has determined compliance with this § [14-701\(5\)](#)(b) (Option A: Sky Plane Controls).³⁸²

Table 14-701-5: Sky Plane Requirements by Street

A. Regulated Street Frontage	B. Height Threshold (ft. above sidewalk)	C. Regulated Height Interval (ft. above sidewalk)	D. Allowed Blockage of Sky Plane	
50-59 ft. wide streets (including but not limited to Race and Locust)	65	>575	0%	
		285-575	60%	
		150-285	70%	
		90-150	80%	
		65-90	85%	
		<65	100%	
60-69 ft. wide streets (including but not limited to Chestnut and Walnut)	80	>690	0%	
		340-690	60%	

		175-340	70%
		115-175	80%
		80-115	85%
		<80	100%
70-79 ft. wide streets (including but not limited to Arch)	100	>825	0%
		415-825	60%
		200-415	70%
		135-200	80%
		100-135	85%
		<100	100%
80-89 ft. wide streets	110	>920	0%
		460-920	60%
		225-460	70%
		150-225	80%
		110-150	85%
		<110	100%
90-99 ft. wide streets	120	>1030	0%
		510-1030	60%
		255-510	70%
		170-255	80%
		120-170	85%
		<120	100%

100 ft. and wider streets (including but not limited to Market and John F. Kennedy)	125	>1145	0%
		570-1145	60%
		285-570	70%
		190-285	80%
		125-190	85%
		<125	100%

(c) Option B: Open Area, Building Width, Spacing, Height Controls.

(.1) Open Area Above Ground Level.

The following standards apply to all CMX-4 and CMX-5 lots. All height measurements shall include mechanical space:

- (.a) The maximum lot coverage of buildings for the first 65 ft. of building height shall be 100%.
- (.b) The maximum lot coverage for portions of buildings above 65 ft. and up to 300 ft. in height shall be 75% of the lot.
- (.c) The maximum lot coverage for portions of buildings above 300 ft. and up to 500 ft. in height shall be 50% of the lot. As an alternative, buildings over 300 ft. and up to 500 ft. in height may be constructed so that the average lot coverage of the building above 65 ft. shall not exceed 60% of the lot.
- (.d) The maximum lot coverage for portions of buildings above 500 ft. and up to 700 ft. in height shall be 40% of the lot. As an alternative, buildings over 500 ft. and up to 700 ft. in height may be constructed so that the average lot coverage of the building above 65 ft. shall not exceed 50% of the lot.
- (.e) The maximum lot coverage for portions of buildings, including mechanical space, above 700 ft. in height shall be 30% of the lot. As an alternative, buildings over 700 ft. in height may be constructed so that the average lot coverage of the building above 65 ft. shall not exceed 40% of the lot.

(.2) John F. Kennedy Boulevard Regulations.

- (.a) For buildings located within 90 ft. south of the south side of John F. Kennedy Boulevard between 15th Street and 24th Street:
 - (.i) The minimum distance between buildings, from a height 65 ft. or greater, shall be 75 ft.

(.ii) Along any lot line which is not a street line and which is parallel to any numbered street, the minimum distance between the building, from a height of 65 ft. or greater, and that lot line shall be 37.5 feet.

(.b) For buildings located within 90 ft. south of the south side of John F. Kennedy Boulevard between 15th Street and the Schuylkill River, the maximum width of the building frontage along John F. Kennedy Boulevard for portions of buildings above 65 ft. in height shall be the lesser of 250 ft. or 66 percent of the block frontage along John F. Kennedy Boulevard.

(.c) For lots fronting on the south side of John F. Kennedy Boulevard between 15th Street and the Schuylkill River with a second frontage along a numbered street or Juniper Street, the maximum building height along the second frontage for the first 15 ft. of building closest to that street frontage shall be 65 ft.

(.3) Market Street Regulations.

(.a) For buildings located within 90 ft. south of the south side of Market Street between 15th Street and 24th Street:

(.i) The minimum distance between buildings, from a height 65 ft. or greater, shall be 75 ft.

(.ii) Along any lot line which is not a street line and which is parallel to any numbered street, the minimum distance between the building, from a height of 65 ft. or greater, and that lot line shall be 37.5 feet.

(.b) For buildings located within 90 ft. south of the south side of Market Street between 15th Street and the Schuylkill River, the maximum width of the building frontage along John F. Kennedy Boulevard for portions of buildings above 65 ft. in height shall be the lesser of 250 ft. or 66 percent of the block frontage along Market Street.

(.c) For lots fronting on the south side of Market Street between Front Street and the Schuylkill River with a second frontage along a numbered street or Juniper Street, the maximum building height along the second frontage for the first 15 ft. of building depth shall be 65 ft.

(.4) Chestnut Street Regulations.

The following standards apply to lots fronting on the south side of Chestnut Street between Front Street and the Schuylkill River:

(.a) The maximum width of a building frontage along Chestnut Street shall be 100 ft. for corner lots and 60 ft. for intermediate lots.

(.b) Except within 130 ft. east of the east side of Broad Street or within 130 ft. west of the west wide of Broad Street, the maximum building height shall be 50 ft. This height limit may be increased to a maximum height of 320 ft. by increasing the height one foot for each foot the building sets back (i.e. along a 45 degree recession plane) from the street line. ³⁸³

(.5) Sansom Street Regulations.

The maximum building height for lots fronting on Sansom Street between Front Street and the Schuylkill River, except within 130 ft. east of the east side of Broad Street or within 130 ft. west of the west side of Broad Street, shall be 320 ft.

(.6) Walnut Street Regulations.

The following standards apply to lots fronting on Walnut Street between Front Street and the Schuylkill River:

- (a) The maximum width of a building frontage along Walnut Street shall be 100 ft. for corner lots and 60 ft. for intermediate lots.
- (b) On the north side of Walnut Street, except within 130 ft. east of the east side of Broad Street or within 230 ft. west of the west wide of Broad Street, the maximum building height shall be 320 ft.
- (c) On the south side of Walnut Street, except within 130 ft. east of the east side of Broad Street or within 230 ft. west of the west wide of Broad Street, the maximum building height shall be 50 ft. This height limit may be increased to a maximum height of 260 ft. by increasing the height one foot for each foot the building sets back (i.e. along a 45 degree recession plane) from the street line.³⁸⁴

(.7) Locust Street Regulations.

The following standards apply to lots fronting on Locust Street between Front Street and the Schuylkill River:

- (a) The maximum width of a building frontage along Locust Street shall be 100 ft. for corner lots and 60 ft. for intermediate lots.
- (b) On the north side of Locust Street, except within 100 ft. east of the east side of Broad Street or within 230 ft. west of the west side of Broad Street, the maximum building height shall be 260 ft.
- (c) On the south side of Locust Street, except within 100 ft. east of the east side of Broad Street or within 230 ft. west of the west side of Broad Street, the maximum building height shall be 50 ft. This height limit may be increased to a maximum height of 260 ft. by increasing the height one foot for each foot the building sets back (i.e., along a forty-five degree recession plane) from the street line.

(.8) Spruce Street Regulations.

The following standards apply to lots fronting on Spruce Street between Front Street and the Schuylkill River:

- (a) The maximum width of a building frontage along Spruce Street shall be 100 ft. for corner lots and 60 ft. for intermediate lots.

(.b) On the north side of Spruce Street, except within 100 ft. east of the east side of Broad Street or within 230 ft. west of the west wide of Broad Street, the maximum building height shall be 260 ft.

Floor Area and Height Bonuses

§ 14-702. Floor Area and Height Bonuses. ³⁸⁶

(1) Purpose.

The intent of the floor area bonus provisions is to encourage certain types of development and the creation of specific amenities in the public interest. These floor area bonuses shall be available in the areas listed, and in return for the provision of design or amenities listed.

(2) Eligibility for Floor Area Bonuses. ³⁸⁷

In order to be eligible for any floor area bonuses pursuant to this section:

(a) The property must be located in the RMX-3, CMX-3, CMX-4, or CMX-5 districts, and property located in those districts shall only be eligible for the bonuses indicated for that district in this § [14-702](#).

(b) If the property is located in the RMX-3, CMX-4, or CMX-5 districts, the property must have frontage on two streets at least 50 ft. wide or three streets at least 20 ft. wide.

(c) If the property is located in the CMX-3 district, it must be located in: ³⁸⁸

(.1) The Center City Commercial District Control Area, described in § [14-502\(2\)\(b\)\(.29\)](#) ³⁸⁹, provided the property must have frontage on two streets at least 50 ft. wide or three streets at least 20 ft. wide;

(.2) The Old City Residential Area, Bridge Approach, described in § [14-502\(2\)\(b\)\(.17\)](#); or

(.3) An area where the Transit Oriented Development (TOD) regulations of § [14-513](#) apply.

(d) Projects may use more than one bonus option unless specifically stated otherwise in this section.

(e) Property located in the /CDO overlay district or the /ECO overlay district shall be eligible for floor area bonuses, provided the subject property is a minimum of three acres and further provided that any bonus category used to earn additional floor area shall not be the same as any bonus category used to earn additional height. ³⁹⁰

(2.1) Eligibility for Height Bonuses. ³⁹¹

In order to be eligible for any height bonuses pursuant to this section, the property must be located in the /CDO overlay district and must be subject to the height restrictions of § [14-507\(6\)\(b\)](#) or the property must be located in the /ECO overlay district and must be subject to the height restrictions of § [14-519\(4\)\(b\)](#).

(3) Floor Area Bonus Options Summary Table.

The following table summarizes the floor area bonus options in this section. In the event of conflict between the provisions of [Table 14-702-1](#) and the text of this Zoning Code, the text shall govern.

Table 14-702-1: Floor Area Bonus Summary

Bonus Category	Additional Gross Floor Area, as Percent of Lot Area (see § 14-701(2) (Residential District Dimensional Tables) and § 14-701(3) (Commercial Districts Dimensional Table) for the maximum allowed base floor area ratios for each district.)			
	RMX-3	CMX-3 as provided in § 14-702(2)(c)	CMX-4	CMX-5
Public Art (§ 14-702(5))	N/A	N/A	50%	100%
Public Space (§ 14-702(6))	N/A	N/A	Up to 200%	Up to 400%
Mixed Income Housing (§ 14-702(7))	N/A	150%	150%	300%
Transit Improvements (§ 14-702(8))	N/A	N/A	Up to 200%	Up to 400%
Underground Accessory Parking and Loading (§ 14-702(9))	100%	N/A	100%	200%
Green Building (§ 14-702(10))	N/A	Up to 100%	Up to 200%	Up to 400%

(3.1) Building Height Bonus Options Summary Table. ³⁹³

The following table summarizes the building height bonus options in this section. In the event of conflict between the provisions of [Table 14-702-2](#) and the text of this Zoning Code, the text shall govern.

Table 14-702-2: Building Height Bonus Summary [394](#)

Bonus Category	Additional Building Height	
	/CDO	/ECO
Public Art (§ 14-702(5))	Up to 12 ft.	Up to 12 ft.
Public Space (§ 14-702(6))	Up to 24 ft.	Up to 24 ft.
Mixed Income Housing (§ 14-702(7))	Up to 48 ft.	Up to 48 ft.
Transit Improvements (§ 14-702(8))	Up to 72 ft.	N/A
Green Building (§ 14-702(10))	Up to 36 ft.	Up to 36 ft.
Trail (§ 14-702(11))	Up to 72 ft.	N/A
Street Extension (§ 14-702(12))	Up to 72 ft.	N/A
Retail Space (§ 14-702(13))	Up to 48 ft.	Up to 48 ft.
Stormwater Management (§ 14-702(14))	N/A	Up to 72 ft.
Through-Block Connection (§ 14-702(15))	N/A	Up to 48 ft.

(4) Maximum Floor Area and Height Bonus Amounts. [395](#)

- (a) If the property is located in the RMX-3 district, the bonuses in this § [14-702](#) may be used in combination to earn additional floor area ratio up to 100% of the lot area.
- (b) If the property is located in the CMX-3 district, the bonuses in this § [14-702](#) may be used in combination to earn additional floor area ratio up to 250% of the lot area. [396](#)
- (c) If the property is located in the CMX-4 district, the bonuses in this § [14-702](#) may be used in combination to earn additional floor area ratio up to 700% of the lot area.
- (d) If the property is located in the CMX-5 district the bonuses in this § [14-702](#) may be used in combination to earn additional floor area ratio up to 800% of the lot area.

(e) If the property is located in the CMX-5 district, and is included in the Center City/University City Floor Area Ratio area (see § [14-701\(3\)](#) (Commercial Districts Dimensional Table), the bonuses in this § [14-702](#) may be used in combination to earn additional floor area ratio up to 1,200% of the lot area. ³⁹⁷

(f) All bonus floor area earned pursuant to this section must be used so that the property (a) complies with all dimensional standards applicable to the property (other than maximum gross floor area or floor area ratio) in § [14-701](#) (Dimensional Standards), including without limitation the maximum height for the property established in that section, and (b) complies with all other provisions of this Zoning Code, unless a specific exception to one or more of those requirements is listed in this Zoning Code.

(g) All bonus height earned pursuant to this section must be used so that the property (a) complies with all dimensional standards applicable to the property (other than maximum height) in § [14-701](#) (Dimensional Standards), including without limitation the maximum gross floor area or floor area ratio for the property established in that section, and (b) complies with all other provisions of this Zoning Code, unless a specific exception to one or more of those requirements is listed in this Zoning Code. ³⁹⁸

(h) The height bonuses in this § [14-702](#) may be used in combination to earn up to: ³⁹⁹

(.1) 144 ft. of building height in the /CDO overlay;

(.2) 84 ft. of building height in the area of the /ECO overlay bounded by 2nd Street, Spring Garden Street, 6th Street, Willow Street, American Street, and Noble Street; and

(.3) 240 ft. of building height in the area of the /ECO overlay bounded by 2nd Street, Noble Street, American Street, Willow Street, 6th Street, and Callowhill Street.

(5) Public Art.

The bonus for public art is a mechanism to expand and enrich the public's experience and enjoyment of buildings and public space and to create buildings and public space that is designed and executed with diverse and high quality materials, activities, and furnishings. In order to comply with this subsection, all of the following conditions must be met:

(a) Qualifying Criteria.

(.1) Qualifying Items.

The items or programs provided to earn this bonus must meet the definition of "On-site Public Art" or "On-site Cultural Programming" in [Chapter 14-200](#) (Definitions). It is not the intention of these requirements to allow decorative, ornamental, or functional elements of the building or public space that are not designed by an artist and created specifically for the site, nor to have landscaped areas or other furnishings or elements required by this Zoning Code, to qualify as part of the public art requirement.

(.2) Commercial Activity Does Not Qualify.

Items, functions, and events for which an admission, viewing, or participation fee is charged, such as theatrical productions, movies, commercial art galleries, exhibits of items for sale; or that are primarily designed to advertise or promote a commercial good or service, do not satisfy the requirements of this § [14-702\(5\)](#) (Public Art).

(.3) Location.

Public art shall be located upon the building or within public spaces created to meet the requirements of § [14-702\(6\)](#) (Public Space), § [14-702\(8\)](#) (Transit Improvements), or § [14-702\(9\)](#) (Underground Accessory Parking and Loading). Public art may be provided within the public sidewalk if authorization from the City is obtained and if the public art does not impair pedestrian movement within the sidewalk.

(.4) Cost.

The applicant shall provide public art at an amount equal to at least 1% of hard construction costs.

(.5) Agreement and Financial Security.

In order to expedite the development review and approval process, an applicant for a floor area bonus pursuant to this § [14-702\(5\)](#) may sign an agreement with the Art Commission committing to provide, install, and endow public art as required by this § [14-702\(5\)](#) and posting financial security at least equal to the cost of the required public art pursuant to § [14-702\(5\)\(a\)\(.4\)](#) (Cost) above. After signing an agreement in a form acceptable to the City and posting financial security in a form acceptable to the City, the applicant shall be deemed to have satisfied the requirements of this § [14-702\(5\)](#) (Public Art) subject to all other requirements of this § [14-702](#) (Floor Area Bonuses).

(.6) Establishment of Endowments.

The method of establishment of an endowment of a trust or other legally binding mechanism to permanently fund cultural programming to meet the requirements of this section shall be approved by the Art Commission.

(b) Approval Criteria.

(.1) In order to meet the requirements of this section, the applicant must have the proposed artist(s), and all public art and/or proposal for cultural programming approved either by the Art Commission, pursuant to its standard review and approval procedures, or through an Art Selection Committee established pursuant to § [14-702\(5\)\(c\)](#) (Art Selection Committee).

(.2) The Art Selection Committee or the Art Commission, depending on the manner of review and approval chosen by the applicant, shall have the final approval of the artist and all public art submitted to meet the requirements of this § [14-702\(5\)\(b\)](#) (Approval Criteria).

(.3) The Art Selection Committee or the Art Commission, depending on the manner of review and approval chosen by the applicant, may, upon request of the applicant, assist in the selection of an artist or artists for the project and/or the selection of an arts coordinator for the project.

(.4) Final determination as to what items, works, materials, media, infrastructure, support services, etc., qualify as public art or as part of the cost of public art, shall rest with the Art Selection Committee or the Art Commission, depending on the manner of review and approval chosen by the applicant.

(c) Art Selection Committee.

(.1) If the applicant chooses to use an Art Selection Committee, it shall consist of five members all of whom shall conform to one or more of the following criteria. At least two members of the committee shall be practicing artists from the visual or performing arts disciplines. All of the remaining members of the committee shall be either (i) an art educator from an accredited college or university, or (ii) a public arts administrator, or (iii) a curator of an institution of art, or (iv) a registered architect or landscape architect, but no more than one of the remaining members may be from each of those disciplines.

(.2) Upon the submission of an Art Selection Committee for review by L&I, the applicant shall also submit the list for proposed committee members to the Art Commission. The Art Commission shall then have 30 days to review the qualifications of proposed committee members and to advise L&I in writing as to its recommendations regarding the qualifications and composition of the committee.

(d) Bonus Floor Area or Building Height. ⁴⁰⁰

The additional gross floor area or building height earned for providing public art is:

Additional Gross Floor Area, as Percent of Lot Area		Additional Building Height	
CMX-4	CMX-5	/CDO	/ECO
50%	100%	12 ft.	12 ft.

6) Public Space. ⁴⁰¹

The public space bonus may be earned by providing (1) open space, public parks and/or public plazas or (2) a public room. Public open space, public parks or public plazas provided to earn a floor area or height bonus shall count towards the required open area.

(a) Criteria for Open Space, Public Parks, and Public Plazas.

A floor area bonus may be earned by providing open space, public parks, and/or public plazas, provided they meet the following minimum standards:

- (.1) The space shall be open to the sky, except for arbors and trellises.
- (.2) The space shall include a minimum of 5,000 sq. ft. of contiguous area, and may include a through-block pedestrian walkway that satisfies the following criteria:
 - (a) The applicant's property must contain at least 50,000 sq. ft. of lot area and shall extend (a) between two dedicated public streets or (b) between a dedicated public street and an existing through block pedestrian connection that terminates in a dedicated public alley.
 - (b) The applicant's property must be located in the area bounded by the south side of Arch Street, the north side of Pine Street, the west side of 8th Street, and the east side of 24th Street.
 - (c) The walkway shall be at least 20 ft. in width and unobstructed and open to the sky, not including trees, seating, and landscaped areas.
 - (d) The through block connection shall meet all public access design and engineering requirements for areas required to be accessible to those with physical impairments under the Americans with Disabilities Act and any implementing regulations adopted by the City.
 - (e) The walkway shall be open to the public at least during the hours of 8:00 a.m. to 9:00 p.m., daily.
 - (f) The walkway shall not be more than three ft. above or below the level of the adjacent sidewalk.
 - (g) A minimum of 50% of each building facade that borders the outdoor walkways, up to a height of eight ft. above ground level, shall be improved with an active retail use or with landscaped areas, murals, or architectural details, including but not limited to windows and entrances.
 - (h) Vehicular traffic and loading are prohibited from accessing or being located along walkways.
- (.3) Walkways shall have pedestrian lighting. The maximum height of the lighting shall be 15 ft. with an illumination level of 2.0 maintained foot-candles. Lighting shall be provided along walkways at 100-foot intervals. Lighting design shall be subject to the requirements of § [14-707](#) (Outdoor Lighting).
- (.4) No more than 50% of the space shall be located more than three ft. above ground level.
- (.5) The space may not be used for parking, loading, or vehicle movements, or for vehicles to access areas used for parking, loading, or other vehicle movements.

- (.6) A minimum of 30% of the space shall be landscaped and may incorporate a water feature. Water features shall meet the following criteria:
- (.a) The feature shall be located in a public space.
 - (.b) The feature may consist of fountains, water walls, water channels, water sculptures, pools, or ponds.
 - (.c) The water feature shall be appropriately scaled and designed to enhance the space.
 - (.d) The water feature shall be in operation from at least April 1 through October 30 each year, between the hours of 8:00 a.m. and 9:00 p.m, daily.
 - (.e) The water feature shall not be counted toward a public art requirement unless it is designed by an artist and is approved through the public art process in § [14-702\(5\)](#) (Public Art).
- (.7) Any space that has more than 3,000 sq. ft. of floor area shall have at least one tree per 1,000 sq. ft. in addition to those required by § [14-705](#) (Landscape and Trees).
- (.8) One linear foot of seating shall be provided for each 30 sq. ft. of space.
- (.9) Pedestrian lighting shall be provided in each public open space, park or plaza. The maximum height of the lighting shall be 15 ft. with an illumination level of 2.0 maintained foot-candles. One light shall be provided for every 500 sq. ft. of public space. Lighting design shall be subject to the requirements of § [14-707](#) (Outdoor Lighting).
- (.10) The space shall be connected to a sidewalk, but may not disrupt the continuity of an existing street wall.
- (.11) The space shall be open to the public at least during the hours of 8:00 a.m. to 9:00 p.m. daily.
- (.12) The public open space, park, or plaza shall be located so as to receive natural light during the day.

(b) Criteria for Public Room.

A floor area bonus may be earned by providing a public room, provided it meets the following minimum standards:

- (.1) The space is within a building that allows for year-round, climate controlled use by the public and that has access to the public street, transit concourses and/or transit stations either directly or through another connecting area open to the public.
- (.2) The area shall not include any area for a through-block pedestrian walkway.
- (.3) The space shall include a minimum of 5,000 sq. ft. of contiguous floor area and shall have a minimum unobstructed height of 24 ft.

(.4) All or part of the space may be located at the level of any transit concourse or transit station to which it has direct access, or at any level between the level of that transit station or concourse and ground level.

(.5) The space may not be used for parking, loading, or vehicle movements, or for vehicles to access areas used for parking, loading, or other vehicle movements.

(.6) The space must include access to a public amenity, such as retail, public restrooms or other attraction. Spaces that simply serve as a lobby for the transit concourse or building tenants do not qualify.

(.7) One linear foot of seating shall be provided for each 30 sq. ft. of space.

(.8) The space shall be open to the public at least during the hours of 8:00 a.m. to 9:00 p.m. daily.

(c) Bonus Floor Area or Building Height. ⁴⁰²

The additional gross floor area or building height earned by providing public space is:

Amount of Public Space as Percentage of Lot Area	Additional Gross Floor Area, as Percentage of Lot Area		Additional Building Height	
	CMX-4	CMX-5	/CDO	/ECO
5-10% of lot area	100%	200%	N/A	N/A
11-20% of lot area	150%	300%	12 ft.	12 ft.
21-30% of lot area	200%	400%	24 ft.	24 ft.

(7) Mixed Income Housing.

(a) Criteria.

A floor area bonus may be earned by providing affordable housing, provided that it meets the following standards:

(.1) At least 10% of a minimum of five residential dwelling units constructed using base floor area ratio shall be affordable, as defined in § [14-702\(7\)\(a\)\(.3\)](#) and § [14-702\(7\)\(a\)\(.4\)](#), below.

(.2) This bonus is not available to developers of subsidized housing where 51% or more of the dwelling units will be affordable as defined by § [14-702\(7\)\(a\)\(.3\)](#) or § [14-702\(7\)\(a\)\(.4\)](#).

(.3) Rental housing shall have total monthly costs (including rent and utility costs) that are affordable to households earning up to 80% of the Area Median Income (AMI) reported by the U.S. Department of Housing and Urban Development for the Philadelphia Metropolitan Fair Market Rent Area.

(.4) Owner-occupied units shall have total monthly costs (including mortgage principal and interest, property taxes, property insurance, and condominium or homeowner association fees) that are affordable to households earning up to 100% of the AMI reported by the U.S. Department of Housing and Urban Development for the Philadelphia Primary Metropolitan Statistical Area.

(.5) Affordable units shall be affordable for a term of not less than 15 years, with future sales within such period governed by a restrictive covenant or long-term ground lease.

(.6) Affordable units shall be constructed and available at the same time as market-rate units and reasonably dispersed throughout the development.

(.7) Affordable units shall be comparable to market-rate units in exterior building materials and finishes; overall construction quality; and energy efficiency.

(.8) A developer may choose to make a payment in lieu of building affordable units. The in lieu payment will be contributed to the Philadelphia Housing Trust Fund and shall be the lesser of:

(a) the amount that a household at 80% of AMI adjusted for household size could afford to pay for purchase of a qualifying unit; or

(b) the average cost of constructing a qualifying unit.

(b) **Bonus Floor Area or Building Height.** ⁴⁰³

The additional gross floor area or building height earned for providing mixed-income housing is:

Additional Gross Floor Area, as Percentage of Lot Area			Additional Building Height	
CMX-3 as provided in § 14-702(2)(c)	CMX-4	CMX-5	/CDO	/ECO
150%	150%	300%	48 ft.	48 ft.

(8) **Transit Improvements.** ⁴⁰⁴

(a) **Criteria.**

A floor area bonus or building height bonus, as applicable, may be earned by improving, connecting to, constructing or extending a rail, subway, or trolley transit station or stop, concourse, or water taxi station, provided that the project meets the following standards:

- (.1) The applicant shall submit a letter from the appropriate transit authority confirming that the transit authority approves the portion of the project that will be constructed on its property.
- (.2) Qualifying improvements made by the applicant may include any of the following:
 - (.a) New access easements or improvements to connecting passageways, mezzanines, or concourse areas;
 - (.b) Constructing a new rail, subway, trolley transit or water taxi station;
 - (.c) Extending the concourse of an existing rail, subway, or trolley transit station or expanding an existing water taxi station;
 - (.d) Permanent structural improvements, not including routine maintenance or repairs, to an existing rail, subway, trolley transit, or water taxi station or stop; or
 - (.e) Streetscape improvements in excess of the minimum code requirements that enhance pedestrian and bicycle connections to an existing rail, subway, trolley transit, or water taxi station or stop. Streetscape improvements provided to earn the transit improvement bonus shall be approved by the Commission for the provision of safe and attractive pedestrian and bicycle routes to transit.
- (.3) For the /CDO overlay district, transit improvements may be located within 1,320 ft. of the district.
- (.4) The applicant shall post a bond or letter of credit in an amount equal to 100% of the estimated cost of constructing the transit improvements in a form and with financial entities acceptable to the City or its designee. The required transit improvements must be completed upon issuance of the Certificate of Occupancy for the first permanent structure on the property, and the bond or letter of credit must be kept in force until the completion of the required improvements.

(b) Bonus Floor Area or Building Height.

The additional gross floor area or building height earned by providing transit improvements is:

Additional Gross Floor Area, as Percentage of Lot Area		Additional Building Height
CMX-4	CMX-5	/CDO
50% of lot area per each 1% of	100% of lot area per each 1% of	12 ft. per each 1% of estimated

estimated building hard costs spent on transit improvements, up to a maximum of 200% of lot area	estimated building hard costs spent on transit improvements, up to a maximum of 400% of lot area	building hard costs spent on transit improvements, up to a maximum of 72 ft.
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(9) Underground Accessory Parking. ⁴⁰⁵

(a) Criteria.

A floor area bonus may be earned by providing underground accessory parking, regardless of whether that accessory parking is required by [Chapter 14-800](#) (Parking Design Standards). The intent of this section is not to expand the supply of parking in the RMX-3, CMX-4, or CMX-5 districts, but to encourage parking permitted by this Zoning Code to be constructed underground rather than at surface level or above ground. The bonus is available if the project meets the following standards:

- (.1) At least 75% of the accessory parking constructed shall be for the use of residents or occupants of the project, and no more than 25% of the spaces shall be rented on a daily, weekly, monthly, long-term, or any other term to any non-resident or non-occupant of the premises. This shall not prohibit the building owner from billing building residents and occupants for parking spaces separately from rent or other expenses.
- (.2) All of the accessory parking constructed shall be located below grade.
- (.3) The underground parking constructed must contain a gross floor area (including ramps) equal to at least 50% of the gross floor area of the ground floor of the primary building on the property where the underground parking is located.
- (.4) None of the bonus space gained from underground parking may be used to construct additional parking.

(b) Bonus Floor Area.

The additional gross floor area earned by providing underground accessory parking is:

Additional Gross Floor Area, as Percentage of Lot Area		
RMX-3	CMX-4	CMX-5
100%	100%	200%

(10) Green Building or Site. ⁴⁰⁶

(a) **Criteria.**

(.1) The applicant has submitted documentation sealed by a LEED-AP professional confirming that the building or site has been designed for certification at the LEED Platinum or Gold levels, or certification by an equivalent green building or neighborhood development rating system as approved by the Commission, and that preliminary review of project design and elements confirm that it will earn enough points to be certified at that level by an appropriate certifying agency.

(.2) The applicant has signed an agreement with the City confirming that the building or site will complete the LEED certification and commissioning process at the Platinum or Gold level, and that if the building or site fails to be commissioned at the level for which the bonus has been awarded, the applicant will either:

(a) Make improvements to the design, equipment, or operation until commissioned at that level, or

(b) Be in violation of this Zoning Code and subject to those penalties provided in § [14-306](#) (Enforcement).

(b) **Bonus Floor Area or Building Height.** ⁴⁰⁷

The additional gross floor area or building height earned for a certified green building is:

Level of LEED Certification	Additional Gross Floor Area, as Percentage of Lot Area			Additional Building Height	
	CMX-3 as provided in § 14-702(2)(c)	CMX-4	CMX-5	/CDO	/ECO
Gold	50%	100%	200%	24 ft.	24 ft.
Platinum	100%	200%	400%	36 ft.	36 ft.

(11) **Trails.** ⁴⁰⁸

(a) **Criteria.**

A building height bonus may be earned by either (1) constructing trails only or (2) constructing and dedicating trails, provided that the following minimum standards are satisfied. On-site trails that satisfy the minimum standards shall count towards required open area and lot area for the purpose of calculating floor area ratio. Off-site trails shall not count towards required open area

and lot area for the purpose of calculating floor area ratio. On-site and off-site trails shall not count toward the additional building height bonus if the trails are used to qualify for the bonus for the provision of public open space in the /CDO district, as provided in § [14-702\(6\)\(c\)](#).

(.1) Minimum standards for constructing trails:

(.a) The minimum length of the trail shall be equal to one of the following:

(i) If the trail is roughly paralleling the Delaware River, the trail must extend from the downstream to the upstream boundaries of the lot on which the trail is to be constructed.

(ii) If the trail is not roughly paralleling the Delaware River, then the trail must extend from an existing or planned publicly-accessible trail to a boundary of the lot opposite from the existing or planned publicly-accessible trail.

(.b) The trail, exclusive of buffer zones and planting strips, shall be at least 12 ft. wide.

(.c) The trail may be located on-site or off-site within the boundaries of the /CDO district; provided any off-site trails are located on land that is under the same ownership as the development property and are used to calculate a bonus for only one site.

(.d) The trail shall connect to an existing or planned publicly-accessible trail in accordance with the Comprehensive Plan, as approved by the Commission.

(.e) The trail shall have pedestrian lighting. The maximum height of the lighting shall be 15 ft. with an illumination level of 2.0 maintained foot-candles. At least one light shall be provided for each 100 ft. of trail. Lighting design shall be subject to the requirements of § [14-707](#) (Outdoor Lighting).

(.f) The trail shall be unenclosed and shall be designed and located to receive natural light during the day.

(.g) The trail may not be used for parking, loading, or vehicle movements, or for vehicles to access areas used for parking, loading, or other vehicle movements; this subsection shall not apply to emergency vehicles.

(.h) The applicant shall post a bond or letter of credit in an amount equal to 100% of the estimated cost of constructing the required trail improvements in a form and with financial entities acceptable to the City or its designee. The required trail improvements must be completed upon issuance of the Certificate of Occupancy for the first permanent structure on the property, and the bond or letter of credit must be kept in force until the completion of the required improvements.

(.2) Minimum standards for constructing and dedicating trails:

(.a) The trail shall satisfy all the requirements of subsection (.1), above.

(.b) The trail shall be dedicated to, and accepted by, the City or its designee, at no cost to the City, in a form acceptable to the Law Department, but may, at the applicant's request include cross-easements necessary for applicant's tenants, residents, or visitors to have free pedestrian access across the trail to and from the riverfront. After dedication and acceptance, the applicant shall not restrict access to the trail.

(b) Bonus Building Height.

The additional building height earned for trails is:

Additional Building Height	
/CDO	
Constructing trails only	12 ft. for the first minimum required trail segment and 12 ft. for each improved trail segment of 200 linear ft. thereafter, up to a maximum of 48 ft.
Constructing and dedicating trails	24 ft. for the first minimum required trail segment and 24 ft. for each improved trail segment of 200 linear ft. thereafter, up to a maximum of 72 ft.

(12) Street Extensions. ⁴⁰⁹

(a) Criteria.

A building height bonus may be earned by either (1) dedicating only or (2) dedicating and improving lands along the alignment of an existing street, provided that the dedication and improvements meet the following minimum standards.

(.1) For dedications:

(.a) The land must be located on the subject property between the river and the eastern or southeastern end of a river access street.

(.b) The width of the street extension land dedication must be at least equal to the confirmed right-of-way of the existing public street to be extended.

(.c) The dedication of land must be in fee simple ownership, in a form acceptable to the City or its designee, and accepted by the City, at no cost to the City.

(.2) For improvements:

(.a) Any street extension improvements shall be completed to the standards of the Streets Department.

(.b) The applicant shall post a bond or letter of credit in an amount equal to 100% of the estimated cost of constructing the street improvements in a form and with financial entities acceptable to the City or its designee. The required street improvements must be constructed upon issuance of the Certificate of Occupancy for the first permanent structure on the property, and the bond or letter of credit must be kept in force until the completion of the required improvements.

(b) Bonus Building Height.

The additional building height earned for dedication of street extension lands is:

Additional Building Height	
/CDO	
Dedication only	12 ft. for each land dedication of 200 linear ft. up to a maximum of 36 ft.
Dedication and Street Extension Improvement	24 ft. for each street extension segment of 200 linear ft. up to a maximum of 72 ft.

(13) Retail Space. ⁴¹⁰

(a) Criteria.

A building height bonus may be earned for constructing ground floor retail space, provided that all portions of each ground floor facade that abuts a sidewalk or public space and that is not occupied by entrances, exits, or waiting areas shall be designed and constructed to permit a finished ceiling height of at least 10.5 ft. in order to allow occupancy by a use in the retail sales, commercial services, day care, or library and cultural exhibits use category. Retail space provided to satisfy the ground floor use requirements of § [14-507\(4\)](#)(b) or 14-519(3) ⁴¹¹ shall not apply towards the retail space bonus.

(b) Bonus Building Height.

The additional building height earned for construction of retail space is:

Additional Building Height

/CDO	/ECO
12 ft. per each 5,000 sq. ft. of retail floor area provided, up to maximum of 48 ft.	12 ft. per each 5,000 sq. ft. of retail floor area provided, up to maximum of 48 ft.

(14) Stormwater Management. [412](#)

For properties that contain least 15,000 sq. ft. of lot area, the stormwater management bonus may be earned by (1) providing a stormwater open space, (2) managing street drainage, or (3) providing both stormwater open space and managing street drainage. Open space provided to earn a height bonus that meets the requirements of § [14-702\(14\)](#)(a) (Criteria for Open Space) shall count towards the required open area, but shall not be used to satisfy the bonus requirements of § [14-702\(6\)](#) (Public Space).

(a) Criteria for Open Space.

A height bonus may be earned by providing open space that manages stormwater runoff, provided that the space meets the following minimum standards:

- (.1) The stormwater open space shall be located within 20 ft. of Willow Street, but may be contiguous to other integrated surface stormwater management systems.
- (.2) The stormwater open space shall account for at least 75% of the lot area located within 20 ft. of Willow Street.
- (.3) The stormwater open space shall be at least 75% vegetated or landscaped.
- (.4) The stormwater open space shall not be more than 3 ft. above or 3 ft. below ground level.
- (.5) The stormwater open space shall be open to the sky, except for arbors, trellises, trees, seating, and landscaped areas.
- (.6) At least 15% of the stormwater open space shall be publically accessible. In order to qualify as publically accessible, the space must:
 - (.a) Include walkways, seating areas, water features, or other publically accessible elements;
 - (.b) Be open to the public at least during the hours of 8:00 a.m. to 9:00 p.m., daily; and
 - (.c) Have lighting with a maximum height of 15 ft. and a minimum illumination level of 2.0 maintained foot candles. One light shall be provided for every 500 sq. ft. of public space. Lighting design shall be subject to the requirements of § [14-707](#) (Outdoor Lighting).
- (.7) The stormwater open space shall not be used for parking, loading, or vehicle movements, or for vehicles to access areas used for parking, loading, or other vehicle movements.

(.8) Stormwater runoff shall be managed with an integrated surface stormwater management system that includes surface features for stormwater management, promotes infiltration of stormwater runoff to the maximum degree feasible, and provides for stormwater runoff storage when infiltration is not feasible. Any on-site stormwater runoff that cannot be managed in the stormwater open space shall be managed in additional on-site stormwater management systems.

(.9) The Water Department shall confirm that all on-site stormwater management systems meet the applicable stormwater management requirements of this § [14-702\(14\)](#) and all other applicable stormwater management requirements.

(b) Criteria for Managing Street Drainage.

A height bonus may be earned by managing the street drainage in the stormwater open space or through other stormwater management systems on the property, provided that the Water Department confirms that the stormwater management system meets the applicable stormwater management requirements of this § [14-702\(14\)](#) and all other applicable stormwater management requirements.

(c) Bonus Building Height.

The additional building height earned by providing certain stormwater management systems is:

Open Space Management Requirements	Additional Building Height
	/ECO
The stormwater open space meets the requirements of § 14-702(14)(a) .	24 ft.
Street Drainage Management Requirements	Additional Building Height
	/ECO
The stormwater open space meets the requirements of § 14-702(14)(b) .	12 ft. for every 20% of street drainage area managed, up to a maximum of 48 ft.

(15) Through-Block Connection. ⁴¹³

(a) Criteria.

A height bonus may be earned by creating a through-block connection within 15 ft. of the former right-of-way of Noble Street between 2nd Street and 6th Street provided that a through-block

connection meets the following minimum standards. Open space provided to earn a height bonus that meets the requirements of this § [14-702\(15\)](#)(a) shall count towards the required open area, but shall not be used to satisfy the bonus requirements of § [14-702\(6\)](#) (Public Space).

(.1) The through-block connection shall extend between two or more public streets and shall, to the greatest extent possible, be aligned to meet existing through-block connections or public streets that exist within 15 ft. of the current or former Noble Street right-of-way.

(.2) In the event that the former right-of-way of Noble Street is not under the same ownership between two public streets, a written agreement between the record owners of the respective lots and any associated plans shall be submitted to L&I with the zoning permit application, confirming that a through-block connection will be constructed by one or more of the owners in conformance with this § [14-702\(15\)](#), [414](#) provided that the bonus shall apply only to a single applicant's zoning permit.

(.3) The through-block connection shall be at least 24 ft. in width and unobstructed and open to the sky, not including arbors, trellises, trees, seating, and landscaped areas.

(.4) The through-block connection shall have pedestrian access and meet all public access design and engineering requirements for areas required to be accessible to those with physical impairments under the Americans with Disabilities Act and any implementing regulations adopted by the City.

(.5) Vehicular traffic and loading are allowed only if the through-block connection meets all applicable vehicular standards or regulations adopted by the City.

(.6) The through-block connection shall be open to the public at least during the hours of 8:00 a.m. to 9:00 p.m., daily.

(.7) A minimum of 50% of each building facade that borders the through-block connection, up to a height of at least 8 ft. above ground level, shall be improved with an active retail use or with landscaped areas, murals, or architectural details, including but not limited to windows and entrances.

(.8) Any portion of the through-block connection that abuts a surface parking area shall meet the requirements of § [14-802\(5\)](#) (Parking Landscape and Screening) as though the through-block connection was a public street.

(.9) The through-block connections shall have lighting. The maximum height of the lighting shall be 15 ft. with a minimum illumination level of 2.0 maintained foot-candles. At least one light shall be provided for each 100 ft. of the through-block connection. Lighting design shall be subject to the requirements of § [14-707](#) (Outdoor Lighting).

(b) Bonus Building Height.

The additional building height earned by providing a through-block connection is:

Through-Block Connection Linear Footage	Additional Building Height
	/ECO
200 linear ft. or less	24 ft.
201 linear ft. to 350 linear ft.	36 ft.
Greater than 350 linear ft.	48 ft.

Form and Design

§ 14-703. Form and Design.

(1) General Purpose.

The general purpose of this section is to:

- (a) Allow full development of properties consistent with the dimensional standards established in § [14-701](#) and the Floor Area Bonuses established in § [14-702](#), while establishing baseline requirements for building and site features that will create stable residential neighborhoods, commercial, and industrial areas.
- (b) Enhance the public realm.
- (c) Ameliorate conflict between existing and new structures.
- (d) Encourage effective development of street frontages and other public elements that enable new projects to add value to existing communities.
- (e) Encourage creative and sustainable design responses to contemporary opportunities.
- (f) Improve the overall design quality of the city through the use of objective standards that can be administered by L&I without the need for individualized design review of projects, except in those cases covered by § [14-304\(5\)](#) (Civic Design Review).

(2) Applicability.

- (a) The standards of this section shall apply to all development, except for the following: ⁴¹⁵
 - (.1) Detached and semi-detached buildings in which the principal use is single-family or two-family residential;
 - (.2) A change in the principal use of an existing structure, provided that the structure is not altered; or
 - (.3) Structures in which the principal use is one of the following use categories or subcategories, as described in [Chapter 14-600](#) (Use Regulations):
 - (.a) Park and open space;
 - (.b) Utilities and services, basic;
 - (.c) Utilities and services, major;
 - (.d) Wireless service facility;
 - (.e) Parking, non-accessory;

(.f) Urban agriculture; or

(.g) Industrial.

(b) Unless otherwise expressly stated, the standards of § [14-703\(4\)](#) (Attached Building Design Standards) apply to each group of five or more attached buildings developed as part of a single development project in any zoning district.

(c) Unless otherwise expressly stated, the standards of § [14-703\(5\)](#) (Multi-Family Residential, Commercial, and Institutional) apply in RM-2, RM-3, RM-4, RMX-3, and Commercial Districts to:

(.1) Multi-family buildings that contain 10 or more residential units; and to

(.2) Lots that are at least 10,000 sq. ft. or buildings with at least 10,000 sq. ft. of gross floor area.

(d) In the case of any conflict between the form and design standards of this section and designs recommended as part of the Civic Design Review process in § [14-304\(5\)](#), the form and design standards of this section shall govern.

(e) In the case of any conflict between the form and design standards of this section and design standards applicable to a particular project because of its location in an overlay district listed in [Chapter 14-500](#) (Overlay Zoning Districts) or because of a use-specific standard in § [14-603](#) (Use-Specific Standards) the provisions of the overlay district or use-specific standard shall govern.

(3) Certification.

Zoning permit applications must, if subject to the provisions of this § [14-703](#) (Form and Design), include a site plan and elevation drawings prepared by a licensed architect or a licensed professional civil engineer that demonstrates compliance with the standards of this § [14-703](#) (Form and Design).

(4) Attached Building Design Standards.

New utility meters must be hidden from view from the street frontage.

(5) Multi-Family Residential, Commercial, and Institutional.

The intent of these standards is to promote development of buildings that enhance their surroundings through facades that contribute to street activity, the safe circulation of pedestrians and bicycles to and around buildings, and reduced visibility of utilities and equipment related to buildings.

(a) Site Design.

(.1) Entries.

Each principal building shall have one or more entry doors facing and visible from an adjacent public street.

(.2) Screening.

Utility equipment, transformers, ground mounted chillers or cooling towers, and loading docks including trash loading facilities shall be located in areas that are not visible from adjacent public streets or nearby residential uses, or shall be screened from view. Screening shall be opaque and may include configuration of buildings, permitted fences, lattice structures, topographical changes, and/or plants. Rooftop-mounted equipment shall be screened from view from adjacent public streets, public spaces, or nearby residential uses through the use of parapet walls, roof design configuration, or equipment screens at least as tall as the equipment being screened.

(.3) Access.

(a) When the lot abuts uses in the parks and open space use categories that are administered by DPR, the site design shall provide access for building occupants to the adjacent parks and open space use if appropriate and permitted by DPR.

(b) Where the Commission's Philadelphia Pedestrian and Bicycle Plan show a bicycle or pedestrian path or trail abutting the application site, the site design shall provide connections to those paths or trails.

(.4) Multi-Building Projects.

The standards in this § [14-703\(5\)](#) (Multi-Family Residential, Commercial, and Institutional) shall apply to each building that would be considered a principal building as if it were the only building on the lot.

(b) Building Design.

(.1) Facade Articulation. ⁴¹⁶

Facade articulation is required for each building frontage that is greater than 100 ft. wide and that faces a public street 40 ft. or wider. The intent is to ensure that public streets will not be fronted by featureless, monolithic walls. Between the average ground level and a height of 25 ft., a minimum of 30% of the wall area must include one or more of the following features listed in subsections (.a) through (.c).

(a) Ornamental and structural detail;

(b) Projections, recesses, bays, overhangs, or other variations in planes; or

(c) Transparent windows or other transparent glazed area.

(.2) Exposed Side Walls.

The following standards apply to the exposed portion of a wall abutting a side lot line.

(.a) The total height of the exposed portion of a wall abutting a side lot line may be a maximum of 25 ft. in height without any required facade articulation.

(.b) When the exposed portion of a building wall abutting a side lot line exceeds 25 ft in height, the portion of the wall above 25 ft. must have at least 15% of its total surface consisting of articulation that indicates the floor level or implied window openings or other elements and materials to provide visual relief consistent with the architecture of the building and the neighborhood. This may be accomplished by, but is not limited to:

- (.i) blind windows,
- (.ii) material color or texture change,
- (.iii) architectural screens, lattices, or baffles, or
- (.iv) articulated structural spandrels.

CMX-4 Parking Requirements:

	Minimum Required Parking Spaces (spaces per unit/sq. ft. of gross floor area/beds/seats/room)		
	CMX-2/2.5	CMX-3	CMX-4/5
Residential Use Category (as noted below)			
Household Living (except as noted below)	0	3/10 units	0
Multi-Family	0	3/10 units	3/10 units
Caretaker Quarters	0	0	0
Group Living (except as noted below)	1/10 permanent beds	1/10 permanent beds	0
Single-Room Residence	1/20 units + 1; min. 2	1/20 units + 1; min. 2	0
Public, Civic, and Institutional Use Category (as noted below)			
Detention and Correctional Facilities	1/1,000 sq. ft.	1/1,000 sq. ft.	1/1,000 sq. ft.
Educational Facilities	1/1,000 sq. ft.	1/1,000 sq. ft.	1/1,000 sq. ft.
Fraternal Organization	1/300 sq. ft.	1/500 sq. ft.	1/1,000 sq. ft.
Hospital	1/4 bed design capacity	1/4 bed design capacity	1/4 bed design capacity
Libraries and Cultural Exhibits	1/1000 sq. ft.	None for the first 4,000 sq. ft., then 1/1,000 sq. ft.	None for the first 10,000 sq. ft., then 1/1,000 sq. ft.
Religious Assembly	1/10 seats or 1/1,000 sq. ft., whichever is greater	1/10 seats or 1/1,000 sq. ft., whichever is greater	The greater of (a) none for the first 400 seats then 1/10 seats or (b) none for the first 10,000 sq. ft. then 1/1,000 sq. ft.
Safety Services	1/1,000 sq. ft.	1/1,000 sq. ft.	1/1,000 sq. ft.
Transit Station	0	0	0
Utilities and Services, Basic	1/2,000 sq. ft.	1/2,000 sq. ft.	1/2,000 sq. ft.

Utilities and Services, Major	1/2,000 sq. ft.	1/2,000 sq. ft.	1/2,000 sq. ft.
Wireless Service Facility	0	0	0
Office Use Category (as noted below)			
All Office Uses	0	0	0
Retail Sales Use Category (as noted below)			
All Retail Sales Uses	0	0	0
Commercial Services (as noted below)			
Adult-Oriented Service	0	0	0
Animal Services	0	0	0
Assembly and Entertainment	1/10 seats or 1/1,000 sq. ft., whichever is greater	1/10 seats or 1/1,000 sq. ft., whichever is greater	The greater of (a) none for the first 400 seats then 1/10 seats or (b) none for the first 10,000 sq. ft. then 1/1,000 sq. ft.
Building Services	0	0	0
Business Support	0	0	0
Day Care	0	0	0
Eating and Drinking Establishments	0	0	0
Financial Services	0	0	0
Funeral and Mortuary Services	0	0	0
Maintenance and Repair of Consumer Goods	0	0	0
Personal Services	0	0	0
Radio, Television, and Recording Services	0	0	0
Visitor Accommodations	0	0	0
Vehicle and Vehicular Equipment Sales and Services Use Category (as noted below)			
Commercial Vehicle Repair and Maintenance	2/repair bay or 300 sq. ft. of repair space, whichever is greater	2/repair bay or 300 sq. ft. of repair space, whichever is greater	0

Personal Vehicle Repair and Maintenance	2/repair bay or 300 sq. ft. of repair space, whichever is greater	2/repair bay or 300 sq. ft. of repair space, whichever is greater	0
All Other Vehicle and Vehicular Equipment Sales and Services Uses	1/2,000 sq. ft.	1/2,000 sq. ft.	0
Wholesale, Distribution, and Storage Use Category (as noted below)			
Moving and Storage Facilities	1/1,500 sq. ft.	1/2,000 sq. ft.	1/3,000 sq. ft.
Industrial Use Category (as noted below)			
Artist Studios and Artisan Manufacturing	1/300 sq. ft.	1/800 sq. ft.	None for the first 7,500 sq. ft. then 1/2,000 sq. ft.
Research and Development	1/300 sq. ft.	1/800 sq. ft.	None for the first 7,500 sq. ft. then 1/2,000 sq. ft.
Urban Agriculture Use Category (as noted below)			
Market and Community-Supported Farm	None if lot area is less than 5,000 sq. ft., otherwise 2	None if lot area is less than 5,000 sq. ft., otherwise 2	None if lot area is less than 5,000 sq. ft., otherwise 2

CTR Center City Overlay District – Vine Street Area Requirements

Allowable signs in the Vine Street Area are limited to the following (See Sign Regulations Area Map 1):

(.1) Wall Signs.

Accessory and non-commercial wall signs are permitted at a ratio of two sq. ft. of sign area per lineal foot of street frontage. The top of such wall signs may not extend above the bottom of the second floor of the building on which it is located.

(.2) Freestanding Signs.

Accessory and non-commercial freestanding signs are permitted at a height not to exceed 15 ft. Freestanding signs are permitted a maximum of two sign facings and a maximum of 16 sq. ft. for each sign facing.

(.3) Building Identification Signs.

Building identification signs and non-commercial signs that are located above the bottom of the second floor are permitted upon approval by the Art Commission. The Art Commission has 60 days to approve or disapprove the application, after which its approval will be presumed. The approval of the Art Commission must take into account the impact of the proposed signage on the skyline and view corridors of Center City and the visual aesthetics of the area. The Art Commission shall not approve any sign contrary to the goals and objectives of this Zoning Code.

(.4) Projecting Signs.

Within the area bounded by 9th Street, Winter Street, 11th Street, and Race Street projecting accessory and projecting non-commercial signs are permitted at a ratio of two sq. ft. of sign area per lineal foot of street frontage. Such signs are in addition to the signs permitted in §§ [14-502\(7\)\(e\)\(.1\)](#) through § [14-502\(7\)\(e\)\(.3\)](#) and are subject to Art Commission approval. The Art Commission has 60 days to approve or disapprove the application, after which its approval will be presumed.

(.5) Temporary Signs.

Temporary non-commercial signs and real estate signs are permitted, so long as such signs do not exceed a total gross area of 25 sq. ft.

Parking Design Standards

§ 14-803. Motor Vehicle Parking Standards.

📖(1) Accessory Parking Standards.

(a) Permitted Types.

Accessory parking may be provided as follows:

- (.1) A surface parking lot, provided that mechanical access surface parking lots are prohibited in all zoning districts.
- (.2) A parking garage, including a mechanical access parking garage.
- (.3) Private residence parking garages and carports, provided that: ⁴⁵¹
 - (.a) Private residence garages and carports may not exceed a maximum depth of 25 ft. and a maximum width of 35 ft.
 - (.b) Private residence garages and carports with flat or shed roofs are subject to a maximum height limit of 10 ft. Private residence garages and carports with gable, hip or gambrel roofs are subject to a maximum height limit of 15 ft.

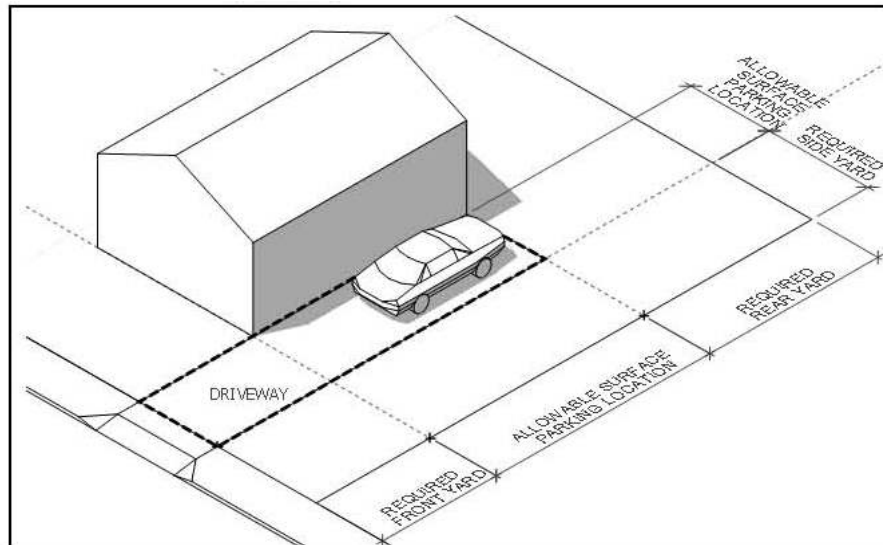
(b) Location of Accessory Parking.

(.1) Residential and Commercial Districts.

(.a) General.

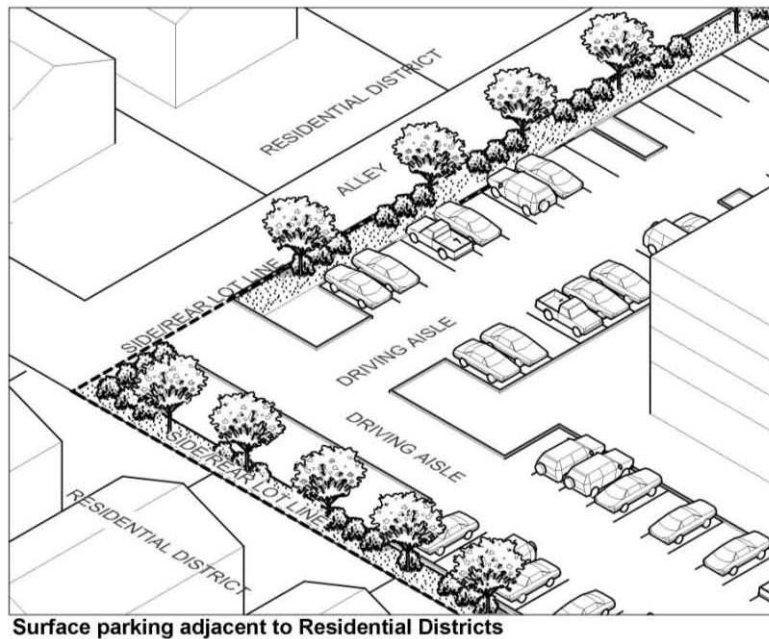
Except as specified in § [14-803\(1\)\(b\)\(.1\)\(b\)](#) (Exceptions) below, accessory parking in Residential and Commercial Districts must comply with the requirements in this § [14-803\(1\)\(b\)\(.1\)\(a\)](#).

- (.i) Garages and carports which are attached to the principal building shall be governed by the same setback and yard provisions as the building itself. ⁴⁵²
- (.ii) Surface parking spaces and detached garages and carports are prohibited in required front, side, and rear yards.
- (.iii) Driveways that provide vehicular access to accessory parking spaces may be located in required front, side, or rear yards.



(.b) Exceptions.

- (.i) Residential parking shall be permitted in the required rear yard of an attached building where the lot is served by a rear alley or shared driveway with a minimum width of 12 ft.
- (.ii) Private residence garages and carports are permitted in any part of the rear yard provided that:
 - (.aa) The permitted occupied area of the lot is not exceeded;
 - (.bb) Any private residence garage or carport erected after June 23, 1998 shall maintain a minimum setback of three ft. between the garage or carport and any property line; and ⁴⁵³
 - (.cc) Any private residence garage or carport erected after June 23, 1998 shall be set back at least 18 ft. from the edge of the sidewalk (or if there is no sidewalk, then the street) closest to the residence.
- (.iii) In the CMX-2.5 district, accessory parking is not permitted between the building line and the street line.
- (.iv) In Commercial districts, where a building with a primary use in the public, civic, and institutional, office, retail sales, or commercial services use categories is located on a lot whose side or rear lot line is abutting a Residential district, no more than one driving aisle with parking spaces on both sides of the aisle may be located between the primary structure and any lot line boundary abutting the Residential district.



Surface parking adjacent to Residential Districts

(.2) Industrial Districts.

Surface parking lots or parking structures in any Industrial district may be located anywhere upon the lot, except in required front yards in the IRMX, ICMX, and I-1 districts.

(c) Additional Regulations for RSA-5, RM-1, CMX-2, and CMX-2.5 Districts. ⁴⁵⁴

Notwithstanding the provisions of this Zoning Code, accessory parking for any single-family, two-family or multi-family use in an attached or semi-detached building in the RSA-5, RM-1, CMX-2, and CMX-2.5 districts shall be prohibited unless it can be accessed from a shared driveway, alley, or rear street.

(c.1) Additional Regulations for the CMX-3 District. ⁴⁵⁵

Notwithstanding the provisions of this Zoning Code, in the CMX-3 district, the vehicular entryways of private residence parking garages and carports shall be prohibited from fronting on any legally opened street.

(d) Additional Regulations for RMX-3, CMX-4, and CMX-5 Districts. ⁴⁵⁶

In the RMX-3, CMX-4, and CMX-5 districts, special exception approval is required for the following:

- (.1) Surface accessory parking lots.
- (.2) Aboveground accessory parking garages.
- (.3) New accessory parking areas.

(.4) Additions to existing accessory parking areas.

(2) Parking Space, Drive Aisle, and Reservoir Space Standards.

The standards of this section apply to all accessory and non-accessory parking areas, regardless of whether such parking is provided to meet minimum off-street parking requirements.

(a) Minimum Area for Driveways and Aisles.

Parking lots and garages, except for those accessory to single-family and two-family uses, must provide an additional area of the parking lot or parking garage equal to not less than 25% of the total area of the lot or garage for access driveways and aisles.

(b) Parking Space and Drive Aisle Dimensions. ⁴⁵⁷

Off-street parking spaces shall comply with the dimensional standards shown in [Table 14-803-1](#) based on the angle of parking and/or parking space type.

Parking Angle/Type	Min. Stall Width (ft.) [1]	Min. Stall Depth (ft.) [2]	Minimum Aisle Width (ft.) [5]
Regular Space – Parallel	8.5	20 for end spaces, 22 for interior spaces	10 ft. for one way traffic 24 ft. for two way traffic
Regular Space – 45 degrees	8.5	18	15 ft. [3]
Regular Space – 60 degrees	8.5	18	18 ft.
Regular Space – 90 degrees	8.5	18	24 ft.
Compact Parking Spaces [4]	8	16	Same as for regular spaces
Spaces for Buildings where occupancy is age-restricted to those 60 years of age or older	10	Same as for regular spaces	Same as for regular spaces
Handicapped Spaces	Shall comply with the Americans with Disabilities Act (42 U.S.C. §§ 12101 et seq.)		

Table Notes:

[1] As measured by a line perpendicular to the stall line at a point on the outside end of the stall, except when the stall is on the inside edge of a curve, in which case the point of measurement shall be on the inside end of the stall.

[2] As measured from required curb on the inside edge of the stall.

- [3] 20 ft. minimum is required where needed as a fire line.
- [4] Limited to 25% of all parking spaces in lots with more than 25 spaces. All compact spaces must be clearly identified.
- [5] Residential uses with 10 or fewer dwelling units are exempt from this provision.

📖 Table 14-803-1: Dimensional Standards for Parking Spaces and Aisles

(c) Reservoir Space.

All surface parking lots and garages in RMX-3, CMX-3, CMX-4, and CMX-5 districts shall provide the number of off-street reservoir spaces required by [Table 14-803-2](#) between the street line and valet pick-up point, or the ticket issuing mechanisms or booths. If the parking lot or garage has two or more points of ingress, the total number of reservoir spaces required at all points of ingress together shall be at least the number required by [Table 14-803-2](#), and there shall be at least one reservoir space at each point of ingress:

📖 Table 14-803-2: Required Reservoir Space

Number of Parking Spaces Provided	Off-Street Reservoir Space Required (7 ft. by 15 ft.)
1 to 50	2
51 to 100	3
101 to 200	5
Over 200 spaces	6 + 1 additional for every 100 spaces over 200

📖 (3) Mechanical Access Parking.

- (a) Mechanical access parking lots are prohibited in all zoning districts.
- (b) Mechanical access parking garages must provide required reservoir spaces.
- (c) Automated valet-type mechanical access parking garages are exempt from any aisle, driveway width, and parking space size requirements.
- (d) Self park-type and tandem parking-type mechanical access parking garages are exempt from parking space size requirements, but must meet any aisle and driveway width requirements.

📖 (4) Parking Design Standards.

- (a) **Design Requirements for Surface Parking Lots.**

Surface parking lots, except those accessory to single-family and two-family uses, shall comply with the requirements of this § [14-803\(4\)](#)(a) (Design Requirements for Surface Parking Lots).

(.1) Grades.

All surface parking spaces shall be situated on ground that does not contain any grades in excess of 10%.

(.2) Materials.

Surface parking lots shall be covered with a hard top surface of cement concrete, bituminous concrete, asphalt, or pervious pavement, including all accessways and/or driveways from the street. If parking is provided in a surface parking lot and the applicant provides more than 20 spaces beyond the minimum off-street parking required by this Zoning Code, all surface spaces beyond the minimum required shall be surfaced with pervious paving materials that meet the design standards of the Philadelphia Stormwater Management Guidance Manual.

(.3) Approval by the Streets Department.

Surface parking lots that provide more than three vehicle parking spaces shall comply with the applicable provisions of § [9-601](#) of The Philadelphia Code and shall be approved by the Department of Streets before L&I may issue a zoning permit.

(.4) Walkways.

Except in Industrial districts, where a parking lot contains more than 25 spaces and is located between a building and a public street, pedestrian walkways at least five ft. wide shall be provided and shall comply with the following standards:

- (a) Each walkway shall be located within or along the edge of the parking lot and shall connect the street and the building.
- (b) One pedestrian walkway meeting these standards shall be provided for each 300 ft. of street frontage.
- (c) Where a walkway crosses a driveway or aisle, the driveway or aisle shall be clearly marked and signed so as to indicate the location of the walkway and to require vehicles to stop for individuals using the walkway.

(.5) Vehicle Access Points. ⁴⁶⁰

Along any street frontage in a Residential district, a surface parking lot shall have no more than one curb cut for both ingress and egress, the maximum width of which shall not exceed 24 ft., or two one-way curb cuts the maximum width of which shall not exceed 12 ft., provided that the curb cuts shall not occupy more than 50% of the street frontage.

(b) Design Standards for Garage Structures. ⁴⁶¹

Accessory and non-accessory parking in garage structures on lots in or adjacent to any Residential district or any Commercial district, except private residence parking garages or those accessory to single-family and two-family uses, shall meet the following standards.

(.1) Vehicle Access Points.

(.a) Along any street frontage, a parking garage shall have no more than one curb cut for both ingress and egress, the maximum width of which shall not exceed 24 ft., or two one-way curb cuts the maximum width of which shall not exceed 12 ft., provided that the curb cuts shall not occupy more than 50% of the street frontage.

(.b) Driveways that cross the public sidewalk must be at the same level as the sidewalk. The driveway material must change at the building line to demarcate the transition to the sidewalk. Sidewalks should be visually continuous across driveways to indicate pedestrians have the right-of-way.

(.2) Facades.

All above ground portions of accessory or non-accessory parking garages or portions of structures occupied by automobile parking shall meet the following standards:

(.a) Facade openings that face any public street or publicly accessible open space shall be vertically and horizontally aligned and all floors fronting on those facades shall be level (not inclined).

(.b) At least 30% of each facade facing a public street or publicly accessible open space shall be occupied by doors (on the ground floor), windows, screens, grills, louvers, or other non-opaque facade elements that resemble those features.

(.c) The remainder of each facade facing a public street or publicly accessible open space shall be designed to conceal the view of all parked cars and internal light sources when viewed from the public street or publicly accessible open space.

(.3) Ground Floor Use.

(.a) In RMX-3 and commercial districts any portion of the ground floor facade that abuts a sidewalk or a public space and that is not occupied by entrances, exits, or waiting areas shall be designed and constructed to permit a finished ceiling height of at least 10.5 ft. in order to allow occupancy by a use in the office, retail sales, commercial services, or public, civic, and institutional use category.

(.b) Uses in the office, retail sales, commercial services, or public, civic, and institutional use category, except the regulated uses that are listed in § [14-603\(13\)](#) (Regulated Uses), are allowed as accessory uses in parking garages that are subject to the ground floor use provisions of § [14-802\(4\)\(b\)\(.3\)\(a\)](#).⁴⁶²

(.4) Parking Garage Review.⁴⁶³

Parking garages in the RMX-3, CMX-3, CMX-4, and CMX-5 districts and parking garages of at least 250 parking spaces on lots in or adjacent to any other residential or commercial district, shall be subject to the following review:

(.a) L&I shall not issue any building permits for parking garages unless the facade of the proposed garage has been reviewed and approved by the Commission. The Commission shall have 30 days upon receipt of an application to review the proposed parking garage; after the 30-day review period has passed, the approval of the Commission shall be presumed.

(.b) L&I shall not issue any building permits for parking garages unless the internal circulation patterns and points of ingress and egress have been reviewed and approved by the Streets Department. The Streets Department shall have 30 days upon receipt of an application to review the proposed parking garage; after the 30-day review period has passed, the approval of the Streets Department shall be presumed.

Table 14-802-4: Required Parking for Persons with Disabilities

Total Parking Spaces Provided	Minimum Number of Accessible Spaces
-------------------------------	-------------------------------------

1 to 5	1 must be provided, but need not be reserved for disabled
6 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9

501 to 1,000	2% of total
more than 1,000	20 plus 1 for each 100 over 1,000

(b) Exceptions.

(.1) Single- and Two-Family Uses.

Single- and two-family uses are exempt from this requirement.

(.2) Multi-Family Uses.

Two percent (2%) of parking spaces provided for multi-family uses that are required by City, state or federal law or regulation to have accessible/adaptable dwelling units shall be accessible. Where 51% or more of parking is provided within or beneath a building, accessible parking spaces shall also be provided within or beneath the building.

(.3) Medical Facilities.

Ten percent (10%) of parking spaces provided for medical outpatient facilities shall be accessible. Twenty percent (20%) of parking spaces provided for medical facilities that specialize in treatment or services for persons with mobility impairments shall be accessible.

(c) Location.

Accessible parking spaces shall be located on the shortest accessible route of travel from adjacent parking to an accessible building entrance. In parking facilities that do not serve a particular building, accessible parking spaces shall be located on the shortest route to an accessible pedestrian entrance to the parking facility. Where buildings have multiple accessible entrances with adjacent parking, accessible parking spaces except for van-accessible parking spaces shall be dispersed and located near the accessible entrances.

(d) Signs.

Designated accessible spaces shall be provided with vertical signage. The sign shall conform to the Federal Highway Administration's "Manual on Uniform Traffic Control Devices." Accessible parking spaces shall also be equipped with the "Reserved Parking Penalties Sign" in accordance with the Pennsylvania Traffic Engineering provisions for Sign R7-8B.

PARKING REDUCTIONS

(8) Adjustments and Alternatives.

The minimum parking requirements listed in this [Chapter 14-800](#) may be adjusted as follows:

(a) Sharing of Parking Spaces.

(.1) Where two land uses listed in separate use categories in § [14-601](#) (Use Categories) share a parking lot, parking lots, or structure, the total off-street parking required for those uses may be reduced by the factors shown in [Table 14-802-6](#). Total off-street parking required shall be the sum of the two parking requirements for the two uses divided by the factors in [Table 14-802-6](#). For example, where a development includes both (a) public, civic, and institutional uses and (b) retail sales uses, the amount of parking required is the sum of the parking required for the two uses divided by 1.3.

(.2) Only those use categories shown in [Table 14-802-6](#) shall be eligible for a reduction under § [14-802\(8\)\(a\)\(1\)](#).

(.3) Where more than two land uses shown in [Table 14-802-6](#) share a parking lot, parking lots, or structure, only the two uses with the greatest off-street parking requirement shall be eligible for a reduction under § [14-802\(8\)\(a\)\(1\)](#).

Table 14-802-6: Shared Parking Reduction Factors
Add the two parking requirements and divide by these factors

Property Use (as stated in § 14-601 (Use Categories))	Multi-Family or Group Living	Public, Civic, and Institutional	Eating & Drinking, Assembly & Entertainment, or Visitor Accommodation	Retail Sales	Office or Commercial Services
Multi-Family or Group Living	–	1.1	1.1	1.2	1.3
Public, Civic, and Institutional	1.1	–	1.2	1.3	1.3
Eating & Drinking, Assembly & Entertainment, or Visitor Accommodation	1.1	1.2	–	1.3	1.7
Retail Sales	1.2	1.3	1.3	–	1.2
Office or Commercial Services	1.3	1.5	1.7	1.2	–

(b) Auto-Share Parking Spaces.

The required minimum number of off-street parking spaces for a residential use may be reduced by four spaces for each automobile parking space reserved as an auto-share parking space, up to a maximum of a 40% reduction in the required minimum number of parking spaces in any one surface parking lot or parking garage. The calculation of maximum surface parking spaces shall not be affected by this reduction.

(c) Reduced Need Populations.

(.1) The required minimum number of off-street parking spaces may be reduced by 33% for any group living use or multi-family use in which occupancy of at least 80% of the units is restricted for use by those 60 years of age or older.

(.2) The required minimum number of off-street parking spaces may be reduced by 50% for any group living use or multi-family use in which occupancy of more than 80% of the units is restricted for use by those meeting the definition of "handicapped" individuals under the federal Fair Housing Act Amendments

(d) Proximity to Transit.⁴⁴⁷

In addition to the parking adjustments permitted in subsections (a) through (c), above, a reduction in the required minimum number of off-street parking spaces is available under § [14-513](#) (/TOD, Transit-Oriented Development Overlay District).

(e) Provision of Bicycle Parking.

For every five Class 1 bicycle parking spaces that are provided on a lot, the number of required off-street automobile parking spaces may be reduced by one space, up to a maximum reduction of 10% of the required automobile parking spaces, provided that the bicycle parking spaces comply with § [14-804\(2\)](#) (Location of Bicycle Parking Spaces) and § [14-804\(3\)](#) (Security).

Bicycle Parking:

§ 14-804. Bicycle Parking Ratios and Standards.

(1) Required Bicycle Parking Spaces.

Bicycle parking spaces shall be provided in accordance with [Table 14-804-1](#):

 **Table 14-804-1: Required Bicycle Parking**

All uses except single-, two-, and multi-family uses, low occupancy facilities, and public parking lots	
Gross Floor Area	Minimum Number of Bicycle Parking Spaces
0 - 7,500 sq. ft.	0
7,501 - 20,000 sq. ft.	2
Over 20,000 sq. ft.	1 per every 10,000 sq. ft. or fraction thereof
Multi-Family Buildings	
Number of Dwelling Units	Minimum Number of Class 1A Bicycle Parking Spaces
Fewer than 12 dwelling units	0
12 or more dwelling units	1 per every 3 dwelling units or fraction thereof
12 or more dwelling units, where building occupancy is age-restricted to those 60 years of age or older	1 per every 10 dwelling units or fraction thereof
Low-Occupancy Facilities	
Number of Employees	Minimum Number of Bicycle Parking Spaces
0 - 5	0

6 - 20	2
21 - 80	4
Over 80	1 per every 20 employees or fraction thereof
Public Parking Lots	
Number of Automobile Spaces	Minimum Number of Bicycle Parking Spaces
0 - 3	0
4 - 20	2
21 - 40	4
41 or more	1 per every 10 spaces or fraction thereof

 (2) **Location of Bicycle Parking Spaces.**

(a) All required bicycle parking on the lot shall be located along an accessible route as defined by the Philadelphia Building Code. Accessible routes shall be provided between the required on-lot bicycle parking and any public bicycle lanes, paths, or routes on adjacent streets.

(b) Bicycle racks may be placed in the public right-of-way to achieve the required number of spaces under this subsection, provided that the building owner enters into a maintenance agreement with the Streets Department and an approval for the installation is obtained from the Streets Department prior to the issuance of any required zoning permit. No additional encroachment ordinance shall be required.

(c) All required bicycle parking spaces outside a building shall be located within a 50 ft. radius of the primary building entrance except in the following circumstances:

(.1) When bicycle parking spaces are to be located on the lot outside a building, L&I may modify or waive this requirement if it would cause a conflict with any other code provision, but all required spaces shall be located as close to the primary building entrance as site conditions allow.

(.2) When bicycle parking spaces are to be located in the public right-of-way, the Department of Streets may modify or waive this requirement if it would result in excessive sidewalk clutter,

impede pedestrian circulation, or cause a conflict with any other code or regulation, but all required spaces shall have convenient access to building entrances that are typically open to intended users.

(.3) Bicycle parking spaces located in attended parking facilities shall be located next to the attendant booth or within an area under regular surveillance.

 (3) **Security.**

(a) Rack elements, or the part of the bike rack that supports one bicycle, shall comply with standards established by L&I and the Streets Department and shall support the bicycle frame at two locations, prevent the bicycle from tipping over, and enable the frame and one or both wheels to be secured with a user-supplied locking device.

(b) If provided, bicycle storage facilities shall be provided with tamper-proof locks. When multiple bicycles are stored together, provisions shall be made for locking bicycles individually.

 (4) **Regulations.**

L&I and the Streets Department are authorized to promulgate such rules and regulations as are necessary or appropriate to implement the provisions of this subsection.

Loading in CMX-4:

(2) **RMX-3, CMX-4, and CMX-5 Districts.**

(a) **Required Spaces.** [472](#)

Off-street loading in RMX-3, CMX-4, and CMX-5 districts shall be provided in accordance with [Table 14-806-2](#), except for:

(.1) Properties in an area bounded by Chancellor Street, 16th Street, St James Street, and 17th Street, where no loading shall be required, provided that the provisions of this § [14-806\(2\)\(a\)\(.1\)](#) shall expire on December 31, 2017.

 **Table 14-806-2: Off-Street Loading in RMX-3, CMX-4, and CMX-5**

Use	Gross Floor Area (Sq. Ft.)	Required Loading Spaces
Office and Visitor Accommodation	100,000-150,000	1
	150,001- 400,000	2
	400,001 - 660,000	3
	660,001 - 970,000	4
	970,001 - 1,300,000	5
	Over 1,300,000	1 additional space per each additional 400,000 sq. ft.
Residential	100,000-200,000	1
	200,001-500,000	2
	Over 500,000	3
All other permitted uses	40,000-100,000	1

	100,001-160,000	2
	160,001-240,000	3
	240,001-320,000	4
	Over 320,000	1 additional space per each additional 90,000 sq. ft.

(b) **Minimum Dimensions.** [474](#)

Loading spaces shall be designed to meet the dimensions in [Table 14-806-3](#).

 **Table 14-806-3: Required Loading Space Dimensions**

Required Loading Space	Dimensions
1	10 ft. wide, 40 ft. long, 14 ft. high
2 [1]	11 ft. wide, 60 ft. long, 14 ft. high
3	10 ft. wide, 30 ft. long, 14 ft. high
4	10 ft. wide, 40 ft. long, 14 ft. high
5 [1]	11 ft. wide, 60 ft. long, 14 ft. high
Each additional	10 ft. wide, 30 ft. long, 14 ft. high

Table Notes:
cept where access to the loading space is by a street which is less than 35 ft. wide in which case, such space may be at least 10 ft. wide, 40 ft. long, 14 ft. high.

(c) **Enclosed Structure Required.**

Every off-street loading space shall be located either (1) within the building it serves, or (2) in an enclosed structure with direct access to each building the loading spaces serve.

(d) **Ingress and Egress.** [476](#)

Each individual loading space or group of loading spaces shall be limited to one two-way curb cut with a maximum width of 24 ft. on the street frontage or two one-way curb cuts with a maximum width of 16 ft. on each street frontage; provided that, for curb cuts on the south side of Fairmount Avenue, between Broad Street and Thirteenth Street, on lots designated "CMX-4," the 24 ft. maximum width shall not apply to a two-way curb cut and a maximum of two two-way curb cuts shall be allowed if the curb cuts are intended to be used for a mixed use development with at least 50,000 gross square feet of space for retail use. As an exception to this standard, when the loading spaces or their access drives have direct access to a street of less than 40 ft. wide, there shall not be a limit imposed on the size of the curb cut(s) to that street. Driveways that cross the public sidewalk must be at the same level as the sidewalk. The driveway material must change at the building line to demarcate the transition to the sidewalk. Sidewalks should be visually continuous across driveways to indicate pedestrians have the right-of-way.

(e) Common Loading.

Two or more buildings may share a common loading area provided that the loading area is located within a commonly accessible structure and that the total number of loading spaces provided is calculated on the basis of the sum of the loading spaces required for each building that the common spaces are intended to serve.

(f) Trash Storage.

All trash storage areas shall be located within the building, within a completely enclosed trash storage area located on the same lot as the building it serves, or within a common loading area as provided in § [14-806\(2\)\(e\)](#) (Common Loading).

📖 (3) General Design and Access Standards.

Except as provided in § [14-806\(2\)](#) (RMX-3, CMX-4, and CMX-5 Districts), the following standards apply to all required off-street loading spaces.

- (a) Where off-street loading spaces do not adjoin the street, convenient access at least 12 ft. wide to each space shall be provided.
- (b) Access driveways across sidewalks shall be subject to the approval of the Streets Department.
- (c) Each required off-street loading space shall be at least 11 ft. wide, 60 ft. long, and have at least 14 ft. of unobstructed height unless otherwise stated in another provision of this Zoning Code for a specific location.
- (d) No required off-street loading space may be located in a required front setback or rear or side yard area.

Exceptions to Dimensional Standards

(6) Exceptions to Dimensional Standards.

The items listed in [Table 14-701-6](#) are exceptions to the dimensional standards otherwise applicable under this § [14-701](#) (Dimensional Standards). In the case of exceptions to yard, setback, and open space requirements, the areas occupied by the encroachments will not be considered to reduce the amount of open space provided.

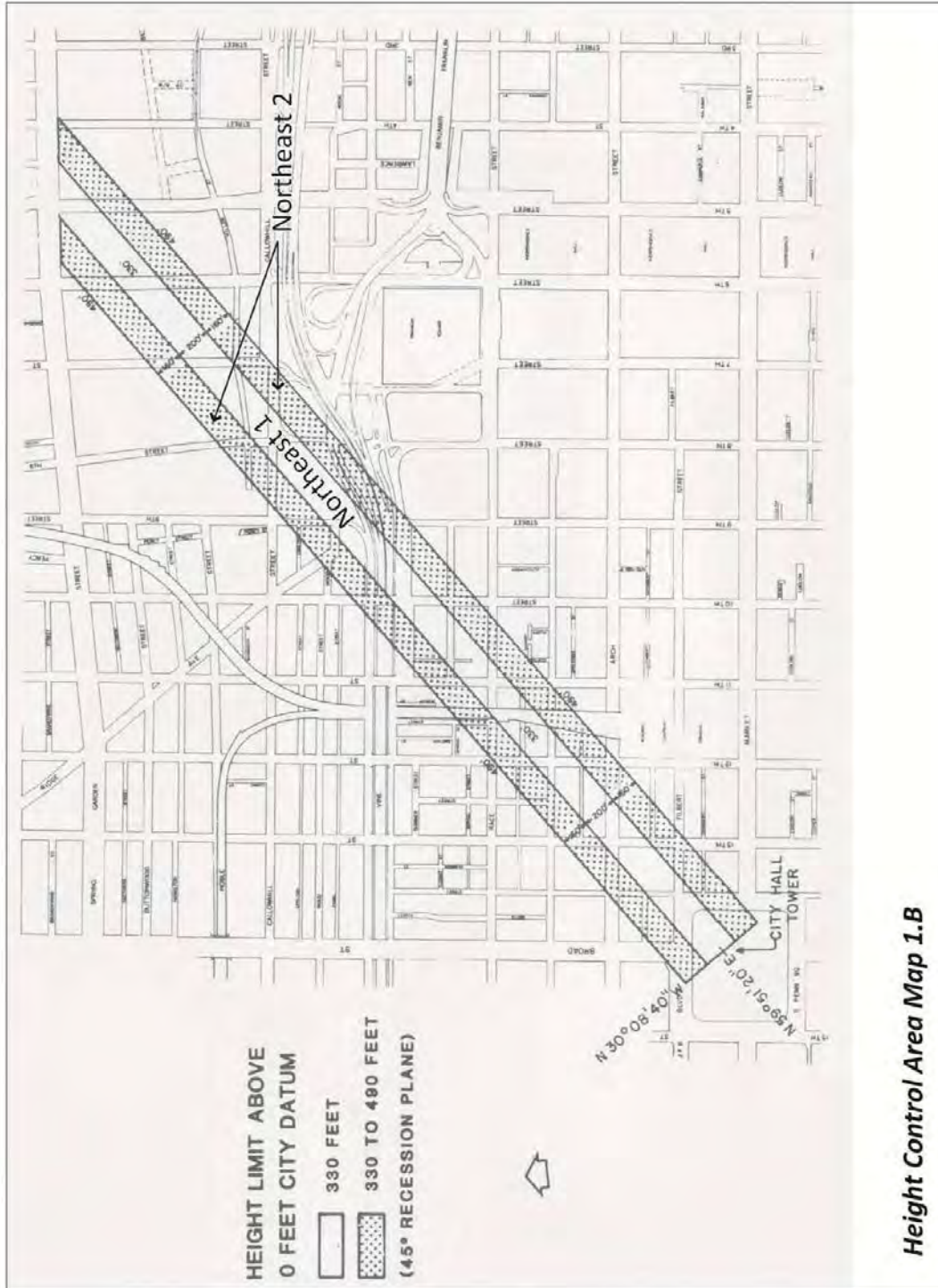
Table 14-701-6: Exceptions to Dimensional Standards

Exceptions to Height Limits	
Type of District	Permitted Encroachment
All Districts	Balustrades and parapet walls may extend above the height limits up to 42 in.
Residential Districts	Monuments, belfries, cupolas, minarets, pinnacles, gables, spires, chimneys, radio or television antennas, fire walls, or ornamental towers not for human occupancy.
	Towers of mechanical or structural necessity where the cross-section area of the tower at no point exceeds 20% of the gross floor area of the ground floor of the building.
	Solar energy collectors: see § 14-604(7) (Solar Collectors).
	Vegetated roof structures (e.g. containers and other permanent infrastructure) that do not extend more than 30 in. above the height limit.
	Wind energy conversion systems: see § 14-604(6) (Small Wind Energy Conversion Systems).
	A roof deck and associated railings and fencing: see § 14-604(5) (Roof Decks).
	Roof deck access structures (e.g., pilot houses): see § 14-604(5) (Roof Decks).
Commercial, Industrial, and Special Purpose districts, when attached to principal building	Chimneys.
	Fire escape towers.
	Penthouses or roof structures for the housing of elevators, stairways, tanks, ventilating fans, heating and cooling towers, or similar equipment required to operate and maintain the building.

	Fire walls.
	Radio, wireless, and television aerials.
	Flagpoles.
	Airplane beacons.
	Solar energy collectors: see § 14-604(7) (Solar Collectors).
	Wind energy conversion systems; see § 14-604(6) (Small Wind Energy Conversion Systems).
	Vegetated roofs structures (e.g. containers and other permanent infrastructure) that do not extend more than 5 ft. above the height limit.
Exceptions to Yard, Setback, and Open Area Requirements	
Type of District	Permitted Encroachment
Residential Districts	Fences are permitted in required front yard areas, but any fence located within a required front setback area shall be no more than 4 ft. tall and no more than 50% opaque. Fence posts and gate posts that are included as architectural features are permitted up to 6 ft. in height. This standard shall also apply to any side of a corner lot facing a street.
	Fences are permitted in the required rear yard area and in any portion of the required side yard area behind the front wall of the principal structure, but may not be more than 6 ft. tall. This standard shall also apply to any side of a corner lot that does not face a street.
	Retaining walls are subject to the restrictions on fence heights to the extent that those walls protrude above the actual ground level at the highest point of the wall, but shall not apply to any retaining wall required by the provisions of any section of The Philadelphia Code or related regulations.
	Entranceway covers, canopies, and awnings may project into required front or rear yard areas up to 5 ft., and may project into required side yard areas up to 3 ft.
	Porches may encroach into the required front setback in RSA-5 and RM-1 districts.
	Balustrades, chimneys, cornices, and vents may project into required front, side, or rear yard or setback areas up to 24 in.
	Chimneys and pilasters may project into required front, side, or rear yard or setback areas up to 18 in.
	Moldings and belt courses may project into required front, side, or rear yard or setback areas up to 18 in.

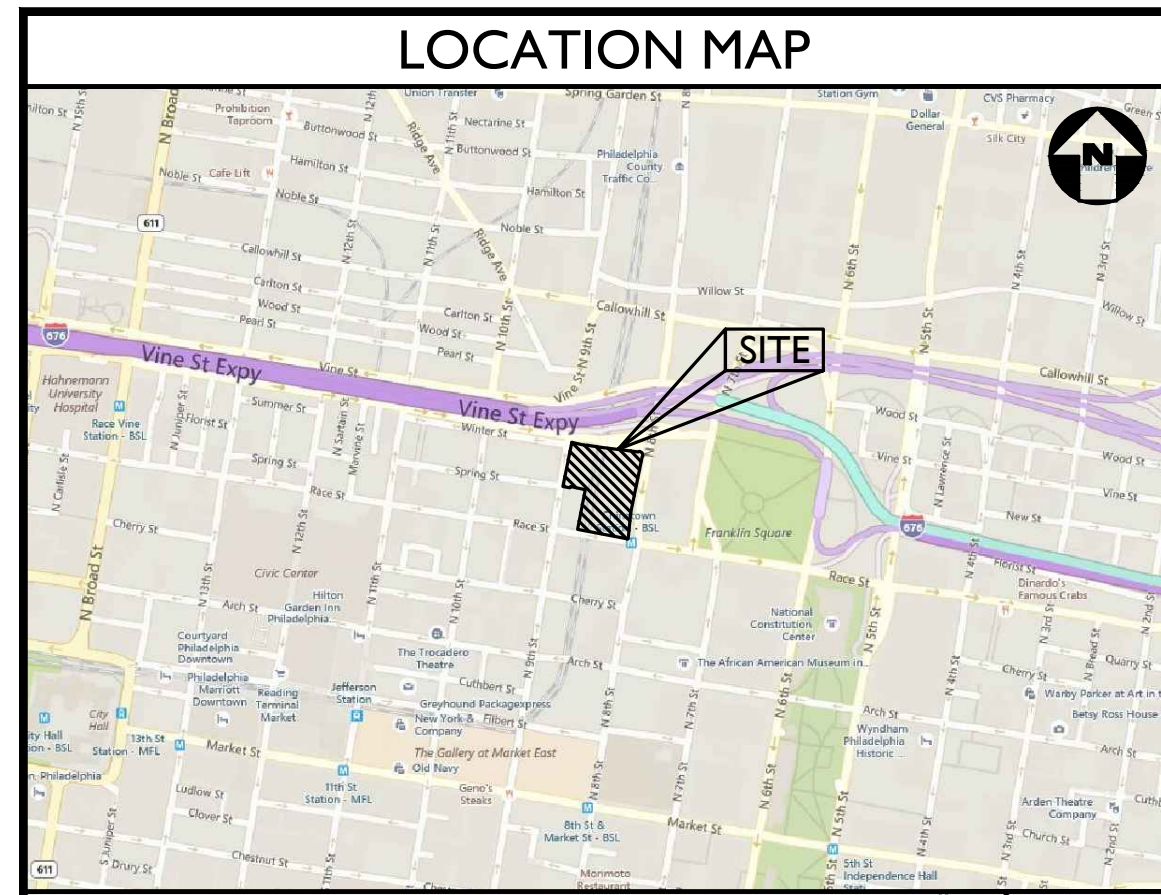
	to 12 in.
	Solar energy collectors: see § 14-604(7) (Solar Collectors).
	Wind energy conversion systems; see § 14-604(6) (Small Wind Energy Conversion Systems).
	Geothermal energy equipment, rainwater harvesting barrels, and odorless composting equipment shall be allowed to encroach into required side yards that are at least 5 ft. wide, and into required rear yards, but not within 2 ft. of any side or rear lot line.
Commercial, Industrial, and Special Purpose districts	Open work fire escapes shall be allowed to encroach into required setback areas.
	In all Commercial districts, areas not included in required occupied areas may not be used for permanent storage purposes, but may be used for temporary storage for up to 30 days in each calendar year. Use of areas not included in required occupied areas for storage for more than 30 days in any calendar year shall require approval of the Zoning Board through the special exception procedures of § 14-303(7) (Special Exception Approval).
	Solar energy collectors: see § 14-604(7) (Solar Collectors).
	Wind energy conversion systems; see § 14-604(6) (Small Wind Energy Conversion Systems).
	Geothermal energy equipment, rainwater harvesting barrels, and odorless composting equipment shall be allowed to encroach into required side and rear yard areas in the Commercial, Industrial, and Special Purpose districts, but not within 2 ft. of any side or rear lot line.

City Hall View Corridor Northeast 2



Zoning Map





SCALE: 1"=1,000' (APPROXIMATE)

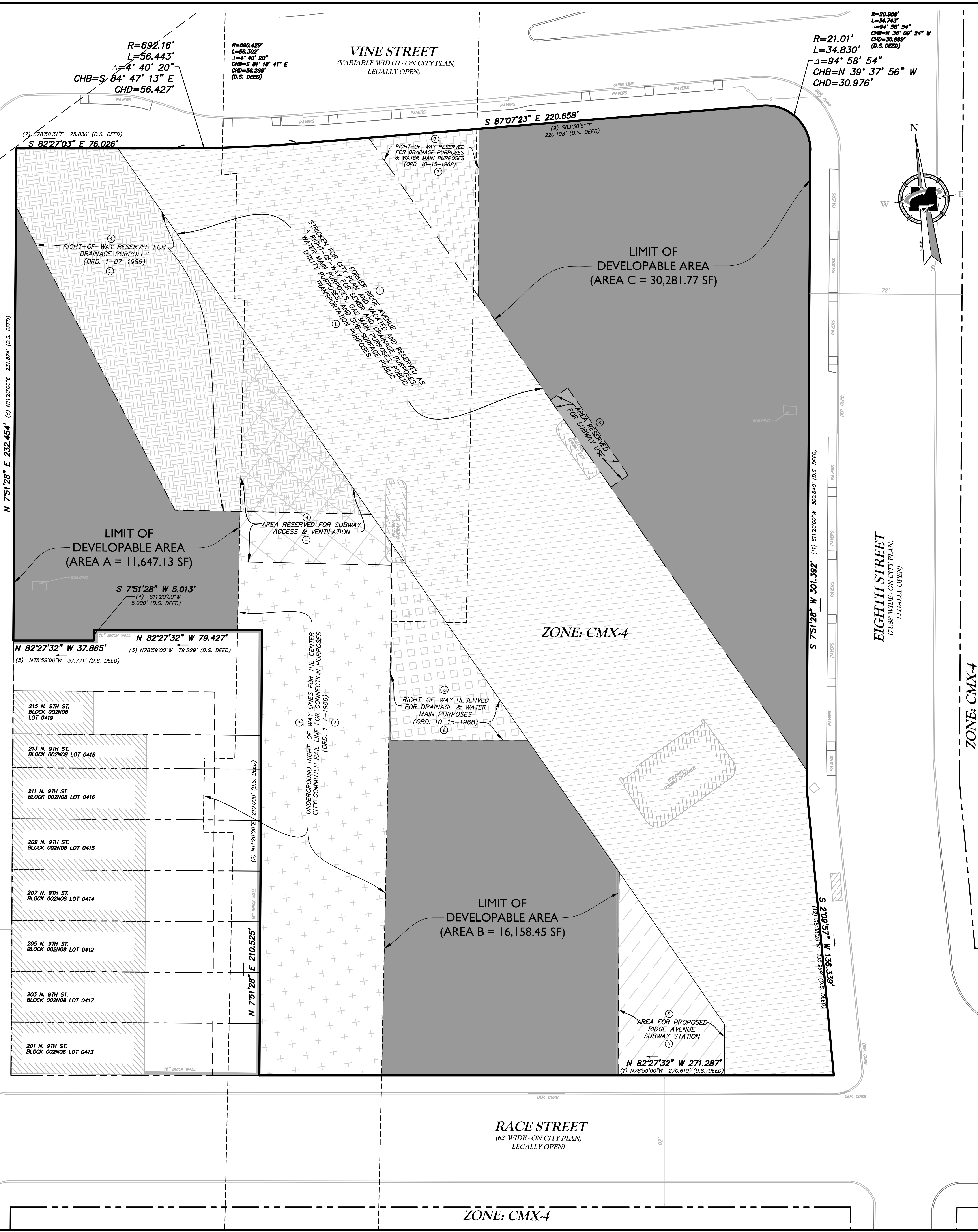
ZONING OVERLAY DISTRICTS: /CTR CENTER CITY OVERLAY DISTRICT CITY HALL VIEW CORRIDOR NORTHEAST 2 (GRAPHICALLY DEPICTED FROM CITY ZONING ORDINANCE)

ZONE: CMX-3

NINTH STREET (65' WIDE - ON CITY PLAN, LEGALLY OPEN)

SPRING STREET (38' WIDE - ON CITY PLAN, LEGALLY OPEN)

ZONE: CMX-4



ZONING REQUIREMENTS

LOCATION: 800-830 VINE STREET, CITY OF PHILADELPHIA, PHILADELPHIA COUNTY, PENNSYLVANIA
 OWNER: PHILADELPHIA REDEVELOPMENT AUTHORITY, 1234 MARKET STREET, 16TH FLOOR, PHILADELPHIA, PA 19107
 EXISTING ZONE: CMX-4 ZONING DISTRICT
 EXISTING USE: PARKING LOT & SUBWAY EXISTENTRANCE
 PERMITTED USE: REFER TO THE CITY OF PHILADELPHIA ZONING CODE

DESCRIPTION	REQUIRED
MIN. DISTRICT AREA	N/A
MIN. LOT AREA	N/A
MIN. STREET FRONTAGE	N/A
MIN. LOT WIDTH	N/A
MIN. LOT DEPTH	N/A
MIN. OPEN AREA	N/A
MAX. OCCUPIED AREA	BUILDINGS ≤ 5 STORIES WITH 1 OR MORE DWELLING UNITS: 10%; OTHERS: 0%
MIN. FRONT YARD BUILDING SETBACK	N/A
MIN. SIDE YARD BUILDING SETBACK	N/A
MIN. REAR YARD BUILDING SETBACK	N/A
MAX. BUILDING HEIGHT	IF USED: BUILDINGS ≤ 4 STORIES WITH THREE OR FEWER DWELLING UNITS = 5; OTHERS = 8'
MIN. PARKING	N/A
MIN. FRONT YARD PARKING SETBACK	N/A
MIN. REAR YARD PARKING SETBACK	N/A
MIN. DRIVEWAYS AND AISLES %	25%
MIN. LOADING SPACES	+
MIN. BICYCLE PARKING SPACES	+
MIN. INTERIOR LANDSCAPE REQUIREMENT (FOR PARKING)	10%

*REFER TO THE CITY OF PHILADELPHIA ZONING ORDINANCE
 **PER THE CODE, THE PROPERTY IS "BOUNDED BY THREE OR MORE STREETS" THEREFORE, ALL YARDS ADJUTTING A STREET ARE FRONT YARDS AND THE REMAINING LOT LINE SHALL BE CONSIDERED A REAR YARD. THE SIDE YARD REQUIREMENTS OF THE ZONING DISTRICT SHALL NOT APPLY.
 ***SUBJECT TO SKY PLANE REQUIREMENTS AND POTENTIALLY THE FOLLOWING: CITY HALL VIEW CORRIDOR NORTHEAST 2: THE MAXIMUM HEIGHT INCREASES FROM 330 FT. ALONG A 45 DEGREE RECEPTION PLANE FROM THE OUTER EDGE OF NORTHEAST VIEW CORRIDOR 1 TO THE OUTER EDGE OF NORTHEAST VIEW CORRIDOR 2, SUCH THAT THE MAXIMUM HEIGHT AT THE OUTER EDGE OF NORTHEAST VIEW CORRIDOR 2 IS 490 FT.

- GENERAL NOTES:
- MASER CONSULTING, P.A.'S BUSINESS PRIVILEGE NUMBER IS 305518.
 - LOT DIMENSIONS & EASEMENT DIMENSIONS ARE BASED ON PHILADELPHIA DISTRICT STANDARDS UNLESS OTHERWISE NOTED.
 - PWD WATERSHED DISTRICT: DELAWARE DIRECT WATERSHED (SOUTH)
 - THIS PROPERTY IS LOCATED IN FLOOD HAZARD ZONE X (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN)
 - THIS PLAN IS INTENDED FOR CONCEPT PURPOSES ONLY AND IS NOT TO BE USED AS A ZONING AND/OR CONSTRUCTION DOCUMENT. ADDITIONAL DUE DILIGENCE AND CITY/STATE APPROVALS ARE REQUIRED TO CONFIRM A FINAL SITE PLAN LAYOUT, INCLUDING FINAL DEVELOPMENT LIMITS.

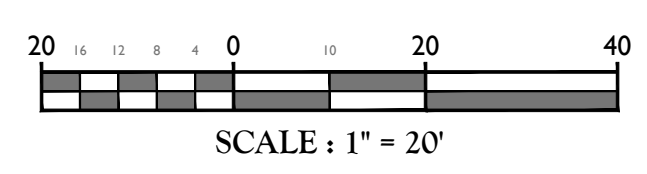
SUMMARY OF RESTRICTION AREAS (USS)	AREA	LEGEND
EXISTING LOT	142,842.58 S.F. (3.279 AC.)	
① FORMER RIDGE AVENUE	47,452.42 S.F. (1.089 AC.)	
② R.O.W. CENTER CITY COMPUTER RAIL LINE	37,899.55 S.F. (0.870 AC.)	
③ R.O.W. FOR DRAINAGE	13,033.23 S.F. (0.299 AC.)	
④ SUBWAY ACCESS AND VENTILATION	4,296.92 S.F. (0.099 AC.)	
⑤ PROP. RIDGE AVE. SUBWAY STN.	3,009.39 S.F. (0.069 AC.)	
⑥ R.O.W. FOR DRAINAGE AND WATER MAIN	2,999.66 S.F. (0.069 AC.)	
⑦ R.O.W. FOR DRAINAGE AND WATER MAIN	2,054.91 S.F. (0.047 AC.)	
⑧ AREA FOR SUBWAY USE	502.50 S.F. (0.012 AC.)	
⑨ AREA FOR DEVELOPABLE AREA A	11,647.13 S.F. (0.267 AC.)	
⑩ AREA FOR DEVELOPABLE AREA B	16,158.45 S.F. (0.371 AC.)	
⑪ AREA FOR DEVELOPABLE AREA C	30,281.77 S.F. (0.695 AC.)	
TOTAL DEVELOPABLE AREA	58,087.35 S.F. (1.333 AC.)	

LEGEND

- TRaverse Line, CENTER LINE OF BASELINE
- RIGHT OF WAY LINE
- PROPERTY LINE
- EDGE OF PAVEMENT
- CURB LINE
- DEPRESSED CURB
- CHAIN FENCE
- METLAND LINE
- MUNICIPAL BOUNDARY
- TREELINE
- ELECTRICAL MANHOLE
- WATER MANHOLE
- TELEPHONE MANHOLE
- UNMARKED MANHOLE
- SANITARY MANHOLE
- DRAINAGE MANHOLE
- MINOR CONTOUR
- MAJOR CONTOUR
- SPOT ELEVATION
- TOP OF CURB ELEV.
- BOTTOM OF CURB ELEV.
- U/S CABLE TV LINE
- U/S FIBER OPTIC LINE
- U/S TELEPHONE LINE
- U/S ELECTRIC LINE
- OVERHEAD WIRE
- WATER MAIN
- GAS MAIN
- SAN. SEWER LATERAL
- SAN. SEWER MAIN
- STORM PIPE
- METLAND MARKER
- TREE
- ROADWAY SIGNS
- TRAFFIC FLOW
- MAILBOX
- TRAFFIC SIGNAL POLE
- POLE MOUNTED LIGHT
- UTILITY POLE
- GLY WIRE
- TRANSFORMER
- FIRE DEPT. CONNECTION
- FIRE HYDRANT
- WATER VALVE
- GAS VALVE
- SANITARY CLEANOUT
- CONCRETE MONUMENT
- CAPPED REBAR/IRON PIPE
- STORM INLET TYPE 'A'
- STORM INLET TYPE 'B'
- STM. DBL. INLET TYPE 'B'
- STORM INLET TYPE 'C'
- STM. DBL. INLET TYPE 'C'
- FLARED END SECTION
- HEADWALL

ABBREVIATIONS

- D.C. = DEPRESSED CURB
- BC = BOTTOM OF CURB
- TC = TOP OF CURB
- BOL = BOLLARD
- GR = GRADE
- MB = MAILBOX
- FF = FINISH FLOOR
- UV = UNKNOWN VALVE
- MV = MANHOLE
- DEP. = DEPRESSED
- CL = CENTERLINE
- FM = PARKING METER
- D.S. = DISTRICT STANDARD
- USS = UNITED STATES STANDARD
- MHW = MEAN HIGH WATERLINE
- MLW = MEAN LOW WATERLINE
- TOP OF WALL
- TW = WATERLINE
- TW = TOP OF WALL
- BW = BOTTOM WALL



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811 PROTECT YOURSELF
 ALL STATES REQUIRE NOTIFICATION OF EXISTING UTILITIES BEFORE ANY PREPARED TO OBTAIN THE EARTH'S SURFACE AND/OR SUBSURFACE.
 Know what you dig.
 Call before you dig.
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REV	DATE	DRAWN BY	DESCRIPTION

REV	DATE	DRAWN BY	DESCRIPTION

RHETT N. CHILIBERTI
 PENNSYLVANIA REGISTERED PROFESSIONAL ENGINEER - LICENSE NUMBER: PE7158

DEVELOPMENT LIMIT PLAN
 FOR
PHILADELPHIA REDEVELOPMENT AUTHORITY
 LOCATION:
 800-830 VINE STREET
 PHILADELPHIA, PA 19107

PHILADELPHIA OFFICE
 2 Penn Center
 Suite 222
 1500 FR. Boulevard
 Philadelphia, PA, 19102
 Phone: 215.861.9021
 Fax: 215.977.2887

SCALE	DATE	DRAWN BY	CHECKED BY
AS SHOWN	8/11/16	RCG	RNC
PROJECT NUMBER:	DRAWING NAME:		
1500140A	C-DVP-LMT		

SHEET TITLE:
DEVELOPMENT LIMIT PLAN

SHEET NUMBER:
 1 of 1

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The **community map repository** should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only landward of 0.0' National Geodetic Vertical Datum of 1929 (NGVD 29). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations tables in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations tables should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Pennsylvania State Plane south zone (FIPSZONE 3702). The **horizontal datum** was NAD 83, GRS80 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the National Geodetic Vertical Datum of 1929. These flood elevations must be compared to structure and ground elevations referenced to the same **vertical datum**. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NINGS12
National Geodetic Survey
SSMC-3, #9202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was obtained from the City of Philadelphia including transportation, hydrography, and political boundaries. These files were rectified in 1998 to curb lines generated from orthophotography flown in March of 1996. Additional information has been derived from other sources.

This map reflects more detailed and up-to-date **stream channel configurations** than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to confirm to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

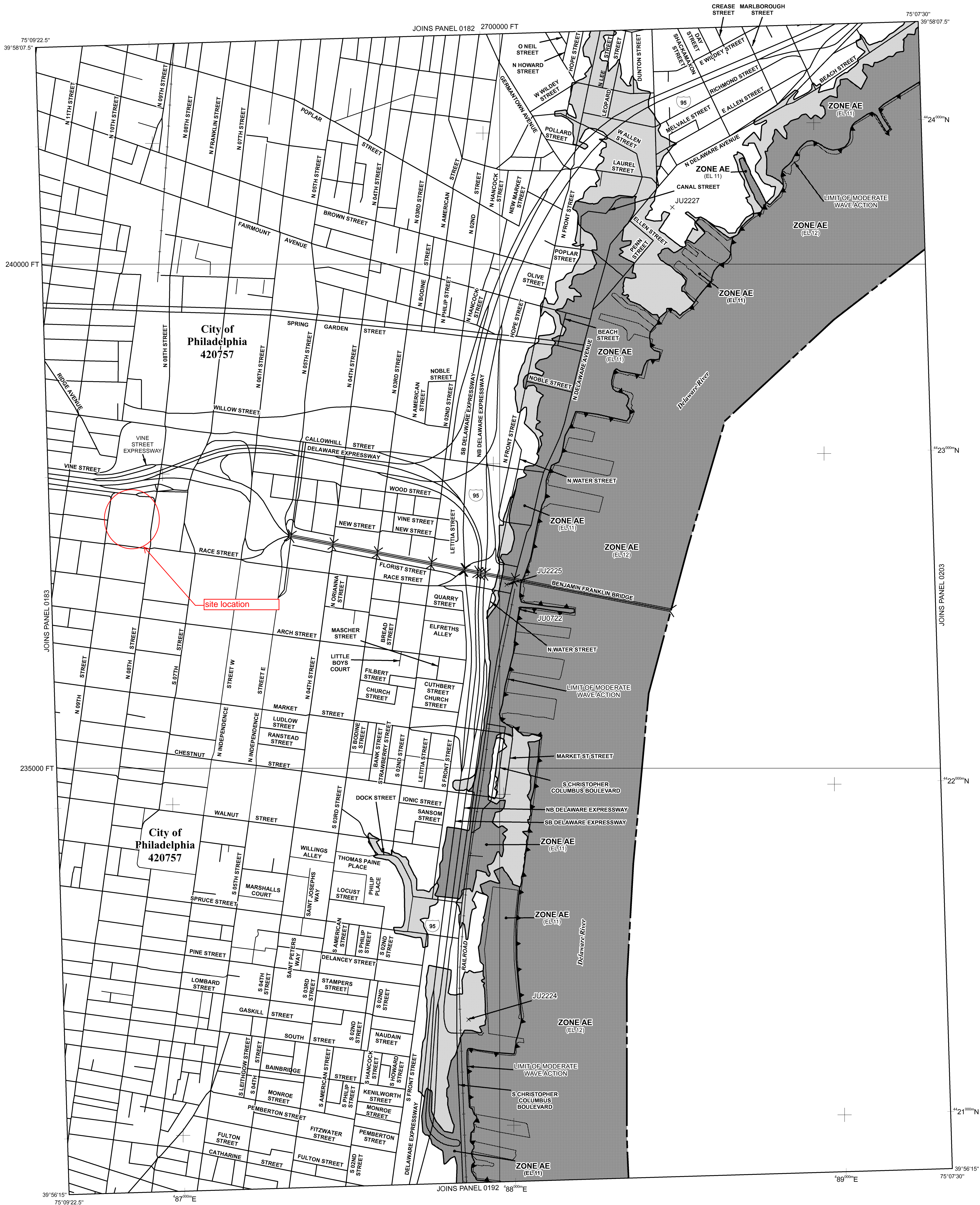
Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed **Map Index** for an overview map showing the layout of map panels for this jurisdiction.

The AE Zone category has been divided by a **Limit of Moderate Wave Action (LiMWA)**. The LiMWA represents the approximate landward limit of the 1.5-foot breaking wave. The effects of wave hazards between the VE Zone and the LiMWA (or between the shoreline and the LiMWA for areas where VE Zones are not identified) will be similar to, but less severe than those in the VE Zone.

For information on available products associated with this FIRM visit the **Map Service Center (MSC)** website at <http://msc.fema.gov>. Available products may include previously issued Letters of Map Change, a Flood Insurance Study Report, and/or digital versions of this map. Many of these products can be ordered or obtained directly from the MSC website.

If you have **questions about this map**, how to order products or the National Flood Insurance Program in general, please call the FEMA Map Information eXchange (FMIX) at 1-877-FEMA-MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov/business/nfp>.



LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V, and VE. The Base Flood Elevation is the water surface elevation of the 1% annual chance flood.

ZONE A No Base Flood Elevations determined.

ZONE AE Base Flood Elevations determined.

ZONE AH Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.

ZONE AO Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.

ZONE AR Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently decertified. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.

ZONE A99 Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.

ZONE V Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.

ZONE VE Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

ZONE X Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.

OTHER AREAS

ZONE X Areas determined to be outside the 0.2% annual chance floodplain.

ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

Floodplain boundary
Floodway boundary
Zone D boundary
CBRS and OPA boundary
Boundary dividing Special Flood Hazard Area Zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, Flood depths or flood velocities.
Limit of Moderate Wave Action

512 (EL 987)
Base Flood Elevation line and value; elevation in feet*
Base Flood Elevation value where uniform within zone; elevation in feet*

* Referenced to the National Geodetic Vertical Datum of 1929

Cross section line
Transect line
Culvert, Flume, Penstock or Aqueduct
Road or Railroad Bridge
Footbridge
Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western Hemisphere
1000-meter Universal Transverse Mercator grid values, zone 18N

600000 FT
5000-foot grid values: Pennsylvania State Plane coordinate system, South zone (FIPSZONE 3702), Lambert Conformal Conic projection
Bench mark (see explanation in Notes to Users section of this FIRM panel)

DX5510 x
River Mile

MAP REPOSITORY
PHILADELPHIA CITY PLANNING COMMISSION, ONE PARKWAY, 13TH FLOOR, 1515 ARCH STREET, PHILADELPHIA, PENNSYLVANIA 19102 (Maps available for reference only, not for distribution.)
INITIAL NFP MAP DATE
December 6, 1974
FLOOD HAZARD BOUNDARY MAP REVISIONS
February 27, 1976
FLOOD INSURANCE RATE MAP EFFECTIVE
June 15, 1979
FLOOD INSURANCE RATE MAP REVISIONS
May 29, 1981; March 15, 1982; February 15, 1992; August 2, 1996; January 17, 2007.
For descriptions of revisions see Notice to Users page in the Flood Insurance Study Report.
November 18, 2015 - to incorporate new detailed Coastal Flood Hazard Analyses.

To determine if flood insurance is available in this community, contact your Insurance agent or call the National Flood Insurance Program at 1-800-438-6620.

MAP SCALE 1" = 500'
250 0 500 1000 FEET
150 0 150 300 METERS

NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0184H

FIRM
FLOOD INSURANCE RATE MAP
CITY OF PHILADELPHIA,
PENNSYLVANIA
PHILADELPHIA COUNTY
PANEL 184 OF 230
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:
COMMUNITY PHILADELPHIA, CITY OF
NUMBER 420757
PANEL 0184
SUFFIX H

Notice to User: The **Map Number** shown below should be used when placing map orders; the **Community Number** shown above should be used on insurance applications for the subject community.

MAP NUMBER
4207570184H

MAP REVISED
NOVEMBER 18, 2015

Federal Emergency Management Agency